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Introduction

Introduction

1.1 How to Read the Built Form Guide

The Built Form Guide (the Guide) is to be read in conjunction with the policies in the Mississauga Official Plan (the Plan) and the Port Credit Local Area Plan (Area Plan). The Guide is to be used during the design and review of development applications.

This Guide demonstrates how the urban form policies in the Plan can be achieved. The Guide is not considered a part of the Area Plan; however, selected content from the Guide has been incorporated into the Area Plan and represents policy. Additionally, applicants must also refer to the principal document, Port Credit Local Area Plan, Zoning By-law, and Building Code and applicable design guidelines and reference notes to ensure that the applicable policies and requirements in these documents have been met. In addition, there may be other City initiatives and directions such as the Old Port Credit Heritage Conservation Plan, Green Development Strategy which need to be consulted.

1.2 Purpose

Building a desirable urban form is a key principle of the Mississauga Official Plan. The Guide is intended to assist in understanding and implementing the Desirable Urban Form policies in the Mississauga Official Plan and the Port Credit Local Area Plan. The Guide establishes and illustrates general requirements necessary to achieve a high quality urban form, site development and public realm.

The Guide is intended to ensure development is appropriate for Port Credit and reflects the unique characteristics of the area. Depending on the context or site, exceptions and variation from the Guide may be considered at the discretion of the City in order to provide some flexibility.

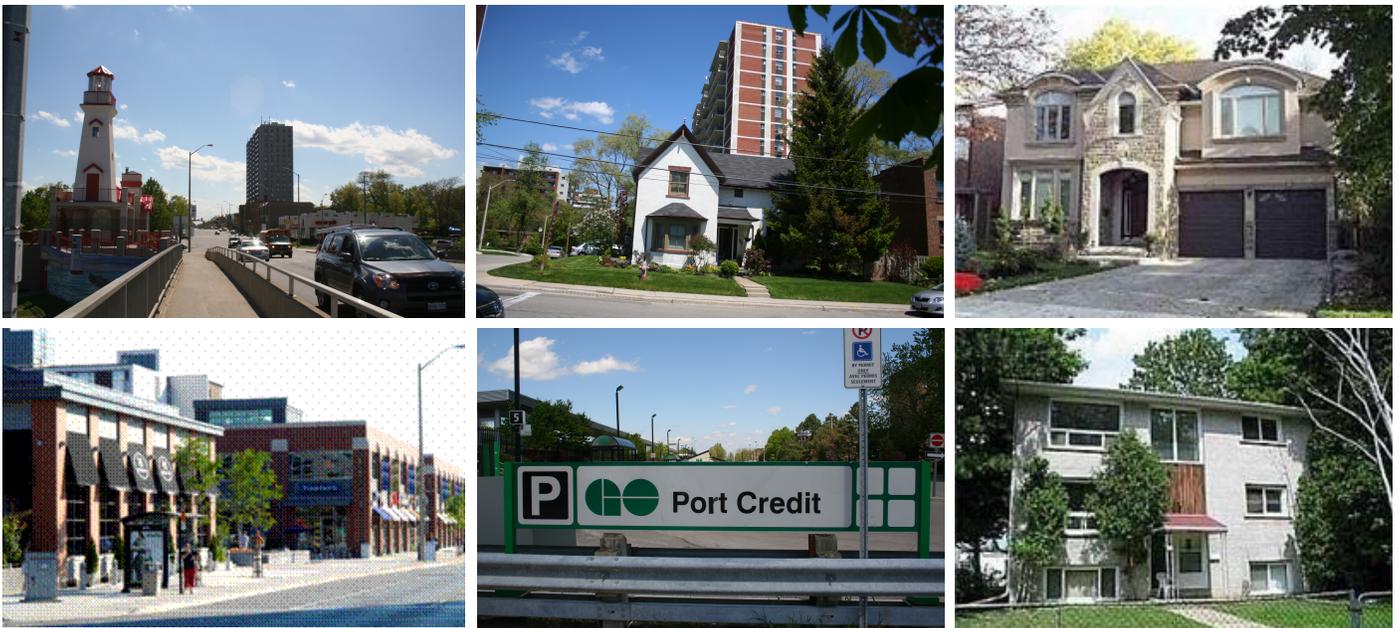


Figure A1 — Port Credit Node and Neighbourhood Images

1.3 Port Credit Local Area Plan

The Area Plan includes lands identified in the City structure as Community Node and Neighbourhood.

Both the Community Node and Neighbourhood Character Areas are divided into precincts which recognize different character attributes of these areas and contain different policy directions and are organized as follows:

Community Node Character Area

The Character Area has been further subdivided into the following precincts:

- Central Residential Area;
- Mainstreet Node;
- Harbour Mixed-Use; and
- Riverside.

Neighbourhood Character Area

The Character Areas have been further subdivided into the following precincts:

- Old Port Credit Village Heritage Conservation District;
- Vacant Former Refinery;
- North Residential Neighbourhood;
- South Residential Neighbourhood; and
- Mainstreet Neighbourhood.

The Guide follows a similar organization, with different sections for the Community Node Character Area and Neighbourhood Character Area. Where appropriate specific direction is given for individual precincts.

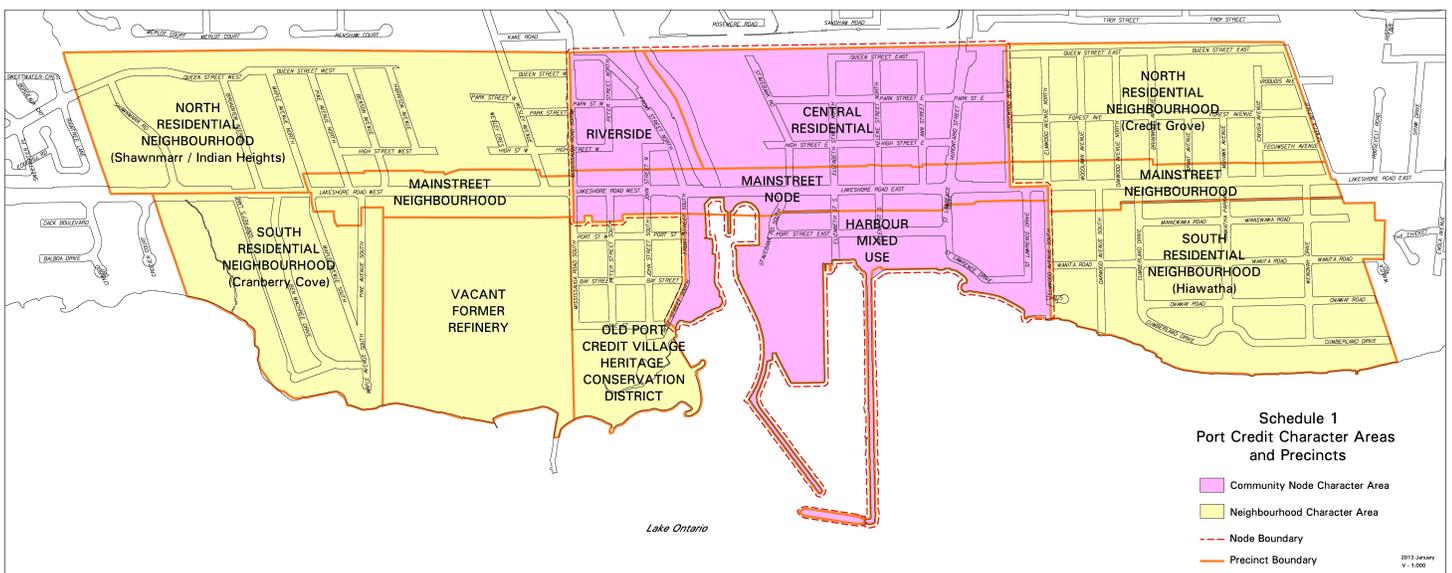
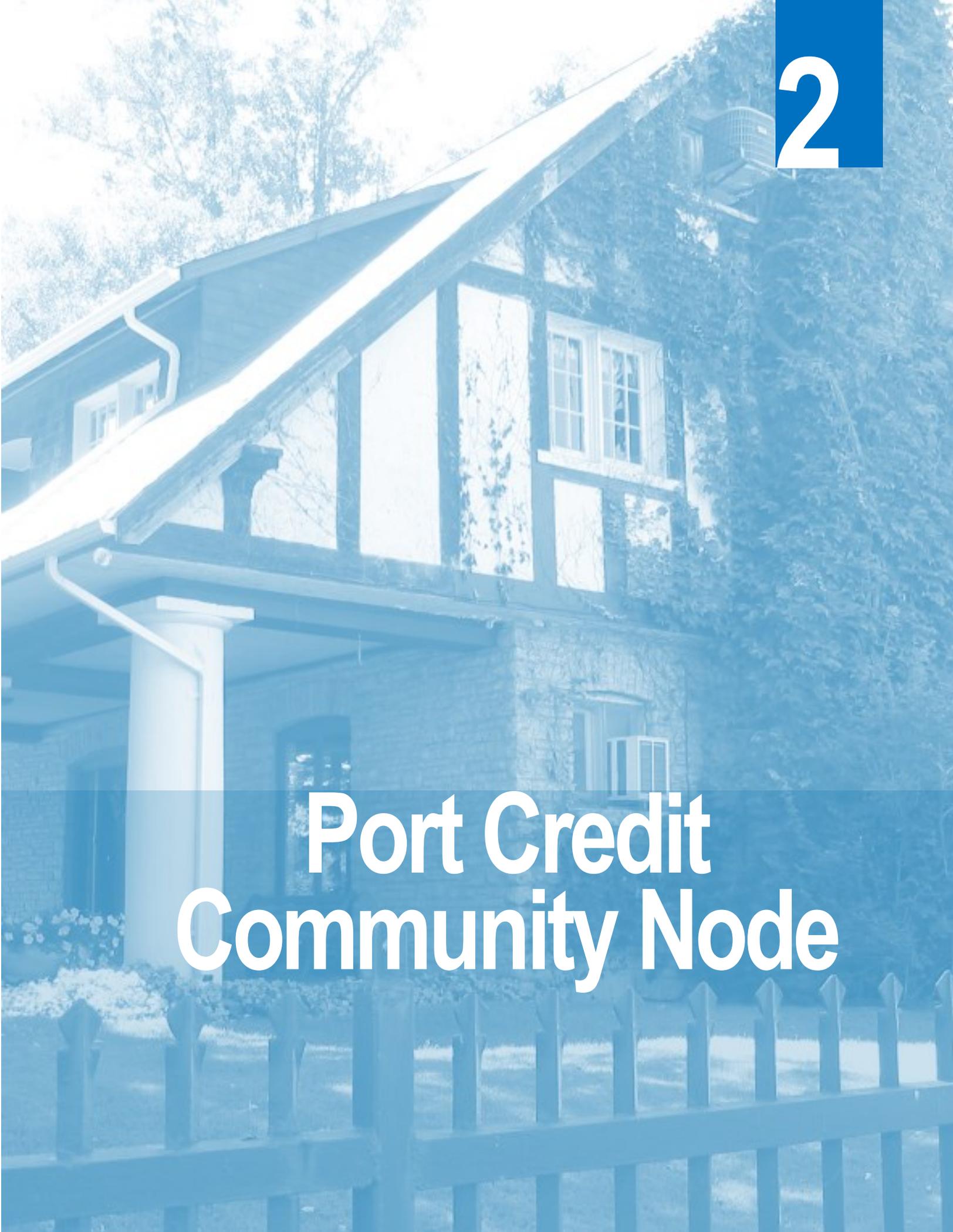


Figure A2 — Port Credit Precinct Map



2

Port Credit Community Node

Port Credit Community Node

2.0 Port Credit Community Node

The Community Node is expected to exhibit a high quality of urban design that should reinforce and enhance the identity of Port Credit as a vibrant and memorable urban place. Additional development is anticipated, however, the form and scale of the development will vary within the Node in accordance with the various precincts. The overall development of the Node shall be at a scale that reflects its role in the urban hierarchy.

The community Node is comprised of the following precincts:

- Riverside Precinct;
- Central Residential Precincts;
- Mainstreet Precinct; and,
- Harbour Mixed Use Precinct

The following provides additional context and information on each of the Community Node Precincts. Subsequent sections provided further direction on specific built form issues.

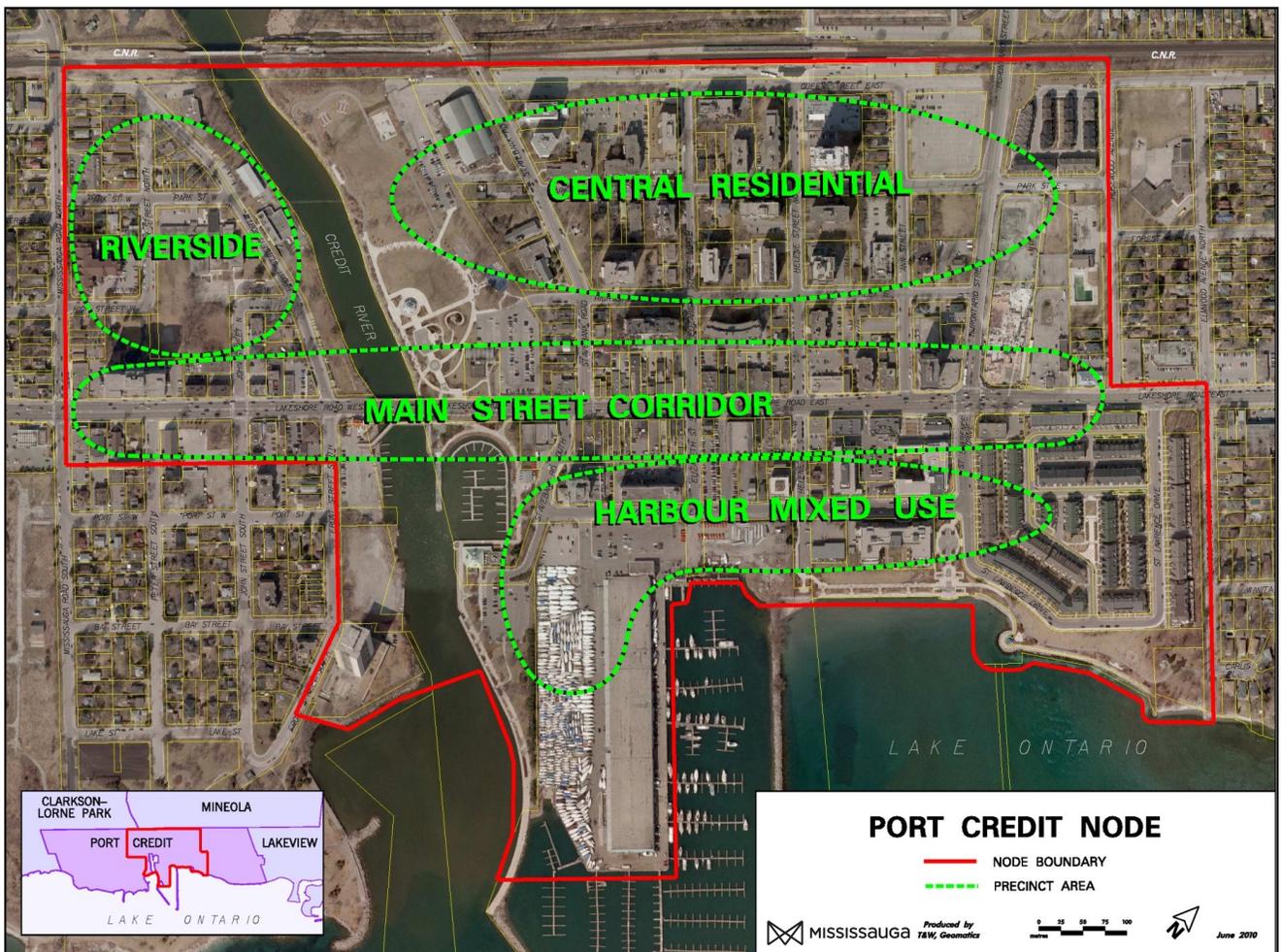


Figure B1 — Port Credit Node Precincts



Figure B2 — Central Residential Precinct Image off of Port Street



Figure B3 — Central Residential Precinct heritage building



Figure B4 — Riverside Precinct Image

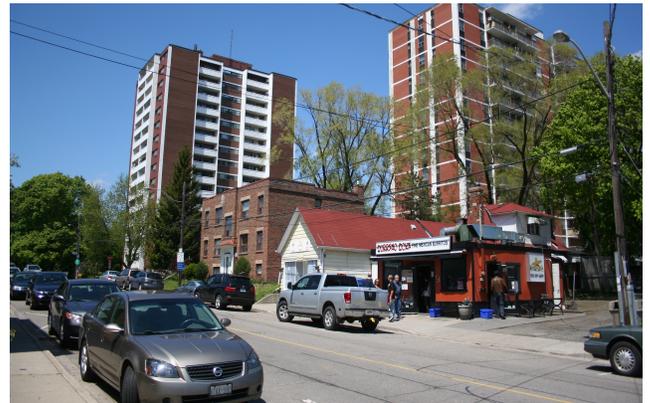


Figure B5 — Central Residential Precinct Image



Figure B6 — Main Street Corridor Precinct (Commercial Area) Image



Figure B7 — Harbour Mixed Use Precinct Image

2.1 Contextual Building Heights

The Port Credit Community Node consists of a mixture of older high rise developments, newer mid-rise developments, older multi-unit walk-ups, as well as detached dwellings.

The majority of the tall buildings are concentrated within the Central Residential Precinct.

There are 33 buildings in the Node between 5 storeys and 27

storeys. The predominant character is buildings ranging from 5 to 16 storeys with taller buildings in key locations.

There are 6 buildings in the Port Credit Node which are taller than 15 storeys:

- 16 storeys—Stavebank Road north and High Street;
- 18 storeys—West Gateway into the Port Credit Node;



DATE OF AERIAL PHOTO: SPRING 2010

PORT CREDIT NODE BUILDING HEIGHTS (5 Storeys and greater)

— NODE BOUNDARY
12 NUMBER OF STOREYS

Note: heights based on city databases and field observations



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T&W, Geomatics



July 2010

Figure B8 — Existing building heights in Port Credit

- 20 storeys—West mouth of the Credit River;
- 27 storeys—at the GO Station entrance; and,
- 20 storeys and 22 storeys—located at the foot of Hurontario Street and Lakeshore Road East.

It should be noted that of these buildings, only the 22 storey structure located at the northeast corner of Hurontario Street and Lakeshore Road East was recently constructed whereas

the remaining are more than 30 years old.

Approximately 30% of the properties between Hurontario Street and Stavebank Road are less than 5 storeys.

The existing setback from the street for developments within the Central Residential area are characterized by ample landscaped area and mature trees, which helps mitigate the perception of height.



Figure B9 — Riverside Precinct



Figure B10 — Main Street Corridor Precinct (Commercial Area)



Figure B11 — Central Residential Precinct



Figure B12 — Harbour Mixed Use

2.2 Planned Building Heights

Proposals for new buildings must take into account the complexities of the broader context within which they are planned, including the existing urban rhythms, local architectural language, the fine grain urban detail and the historic setting.

New buildings should make reference to their surroundings

through footprint, setback, street and building alignment. Aligning tall buildings in key locations can create a strong reference point, which enriches urban legibility and aids in navigation.

The greatest heights in the Node are generally located in the Central Residential Precinct closest to the GO Transit Station.

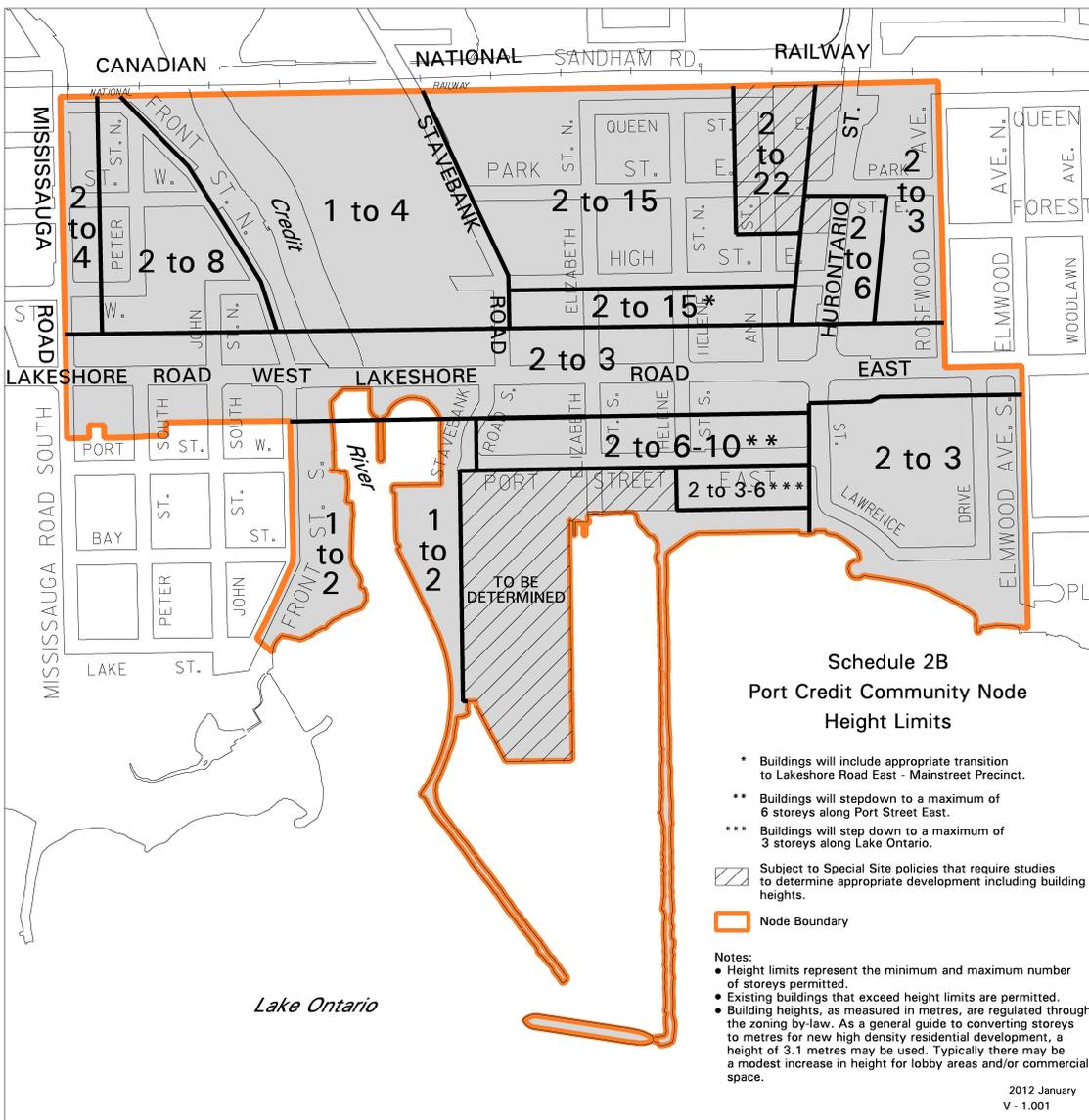


Figure B13 — Maximum Building Heights in the Port Credit Node Precinct

Building heights should generally slope down from the railway tracks to Lakeshore Road East. The highest buildings should be in the vicinity of the GO station and future LRT Station at Park Street and Hurontario Street and then transition downward to Lakeshore Road East and to Lake Ontario and the Credit River.

In general, buildings with the greatest heights should be limited to sites that can be used for way-finding and/or

landmark locations. The maximum height in the Port Credit Community Node shall be 22 storeys which reflects existing building heights in the node and recognizes that 'Community Node' development should not be as high as "Major Nodes". As Community Nodes are intensification areas, all new buildings shall have a height of 2 storeys.

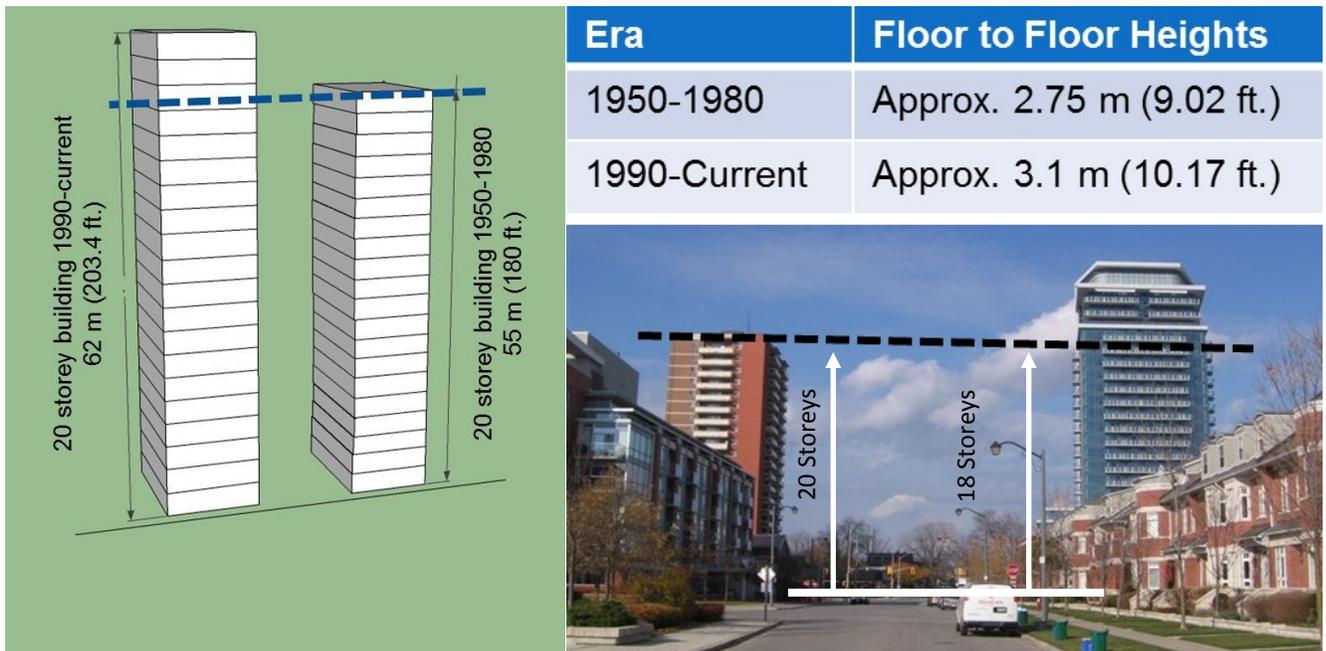


Figure B14 — Differences in Building Heights. Buildings with the same number of floors are approximately 13% taller now.



Figure B15 — Building Transition from the Railway Tracks to Lakeshore Road East and West. Buildings should also transition from Lakeshore Road East and West to Lake Ontario with the exception of key landmark locations (e.g. Intersection of Hurontario Street and Lakeshore Road East)

The majority of the towers in the Port Credit Node were constructed between the post war era and 1980. The dimension of the floor to ceiling heights during this era are significantly lower than those of today (see Figure B14). Consequently, a 20 storey building today is significantly taller than a 20 storey building constructed between the 1950's and 1980's.

A 22 storey building was approved in 2007 for the northeast corner of Lakeshore Road East and Hurontario Street. This building height was determined to be appropriate given its location at a key intersection and gateway into Port Credit which will create a new visual landmark that balances the massing of the 20 storey building just west of Hurontario Street.

In a limited number of situations, existing buildings heights exceed the maximum limits. However, the intent of the Guide and related Area Plan policies is to generally reinforce the prevailing character, as opposed to increasing the overall height of buildings in the area.

The building heights shown on Figure B13 illustrate the minimum and maximum building heights that will be considered for properties if a series of design, land use and technical objectives can be demonstrated by the applicant. Lower heights may be appropriate for specific properties. Maximum height has been applied to areas that particularly require sensitive building step-backs to achieve appropriate height transitions.

The relationship of a building size to the site area and configuration should be considered in order to avoid a building overwhelming its site. In such cases lower heights should be used. New towers should be compatible and characteristic with the existing and proposed neighbouring structures and in terms of their contribution to the skyline.



Figure B16 — Mainstreet Precinct, Mississauga Road and Lakeshore Road West. New construction.



Figure B17 — Building heights in the Central Residential Precinct



Figure B18 — Port Credit Marina, Snug Harbour

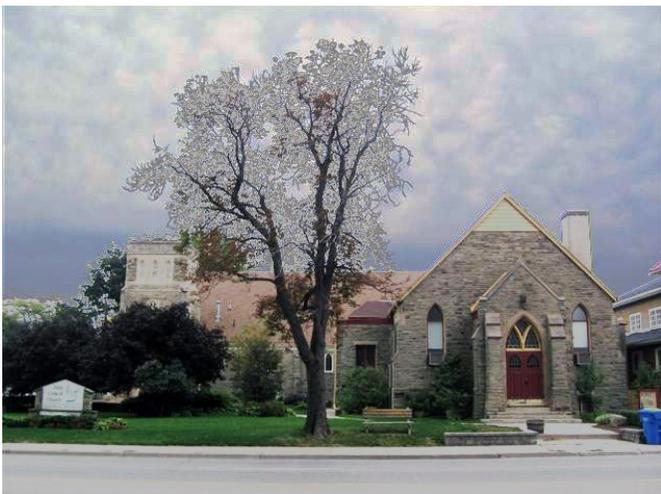


Figure B19 — Mainstreet Precinct on Lakeshore Road West



Figure B20 — Market Square Development, Lakeshore Road East



Figure B21 — Port Credit Node waterfront view



Figure B22 — Port Credit light house

2.3 Community Node Precincts

2.3.1 Riverside Precinct

This Precinct consists primarily of a residential neighbourhood. The area consists of one 18 storey apartment building, a mix of 4 storey walk up apartments, detached and semi detached dwellings, a 4 storey retirement home, a school and some low rise office and commercial uses.

This precinct should create a transition from the building heights of the Central Residential precincts to the low rise building heights in the residential neighbourhoods to the west. The heights in this area should also transition down to the Credit River, the mainstreet precinct and to the stable neighbourhoods to the west of Mississauga Road.



Figure B23 — Image of the Riverside Precinct



Figure B24 — Image of the Riverside Precinct



Figure B25 — Image of the Riverside Precinct



Figure B26 — Image of the Riverside Precinct

2.3.2 Central Residential Precinct

The Central Residential Precinct has the greatest number of apartment buildings ranging from 5 to 27 storeys. Currently, three buildings are higher than 16 storeys (20, 22 and 27) and are located at the foot of Lakeshore Road East and Hurontario Street East, and across from the GO Station.

This area will have the greatest building heights in Port Credit with heights transitioning downward towards the Credit River Valley, the mainstreet precinct and stable neighbourhoods to the east as illustrated in figure B13.

The vicinity between the Go Station and the future LRT stop (on Hurontario Street) has the potential to accommodate the

greatest heights in the area and may have a more urban built form in order to provide a more conducive environment for pedestrians walking between the LRT stop and the GO Station. The specific heights, built form and land uses in this area are subject to further study.

The existing character of the area will generally be maintained including mature trees and well landscaped front yards.

Buildings adjacent to the mainstreet precinct will be required to demonstrate appropriate transition with respect to issues such as sky views, visual impact, and shadow impact.



Figure B27 — Image of the Central Residential Precinct



Figure B28 — Image of the Central Residential Precinct



Figure B29 — Image of the Central Residential Precinct



Figure B30 — Image of the Central Residential Precinct

2.3.3 Mainstreet Precinct

This Precinct includes part of Port Credit's traditional mainstreet, which generally extends a half block north and south of Lakeshore Road East and West. New developments along Lakeshore Road East and West shall be a minimum height of 2 storeys or a height of 7.5 m for any new construction along the Mainstreet Corridor Precinct. A maximum height of 3 storeys or 12 m is permitted as outlined in Figure B31 and B32. The second and third storey must be usable space.

The mixed use component of new development should be continuous for the first half block of Lakeshore Road East and West.

The minimum first floor height of a building along the

Lakeshore Road East and West frontage shall be 4.5 m as indicated in Figure B31.

To ensure buildings and structures relate to human scale and reinforce the scale of the community:

- Built form should be closely related to, and integrated with, the street line, and with minimal building setbacks, to provide spatial enclosure and street-related activity;
- New buildings should be compatible in bulk, massing and scale of the built form to provide an integrated streetscape.
- Retail uses will be required along Lakeshore Road with direct access to the public sidewalk;
- No parking lots or areas should be provided between the building and the street line on principal street frontages,

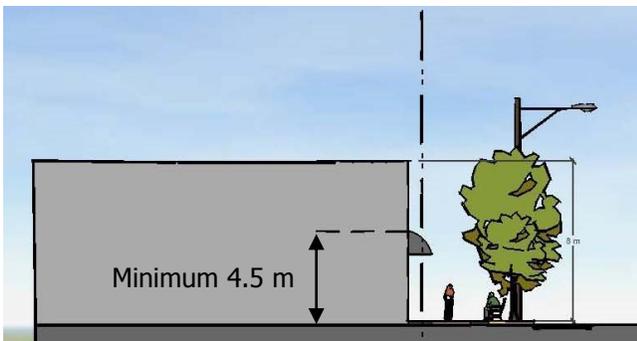


Figure B31 — Minimum building height of 2 storeys will be permitted

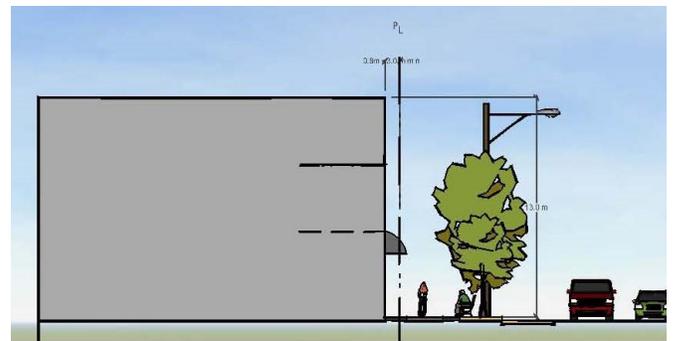


Figure B32 — Maximum building height of 3 storeys or 12 m



Figure B33 — Conceptual 3 storey mainstreet building



Figure B34 — Image of the existing mainstreet building

- with the exception of on-street parking;
- e. Blank walls must be avoided facing principal frontages and intersections;
- f. Service loading and garbage storage should be accessed from the rear or side lanes;
- g. Front building facades should be parallel with the street and provide periodic indentations for visual relief and features such as urban squares;
- h. Signage should be integrated with the scale and character of built form;
- i. Continuity of built form should exist from one property to the next with minimal gaps between buildings;
- J. Buildings adjacent to the Old Port Credit Village Heritage Conservation District should be designed so that they are sensitive to and compatible with the adjacent built form;
- K. For Lands on the South side of Lakeshore Road, between Mississauga Road and the Credit River, The Old Port Credit Village Heritage Conservation District Plan applies.

2.3.4 Harbour Mixed Use Precinct

This area will contain a mixture of uses and densities. Development should be lower in scale than the Central Residential Precinct and step down towards the lake. A minimum height of 2 storeys is required and a maximum height of 10 storeys stepping down to 6 on Port Street and from 6 to 3 storeys towards Lake Ontario will be permitted as outlined in Figure B13.

Mixed uses along Port Street East, Elizabeth Street South and Helene Street South are required. The main floor of all new development should be a minimum of 4.5 m in height to ensure commercial uses can be accommodated on the ground floor.

The Port Credit Harbour Marina Lands are subject to further study to determine appropriate heights, built form and land uses.



(top left) **Figure B35** — Existing development south of Port Street East.
 (top right) **Figure B36** — Existing townhouse development south of Lakeshore Road East



Figure B37 — Existing development south of Port Street East stepping down from 6 to 3 storeys to the Lake.



Figure B38 — Existing development on Port Street East and Stavebank Road South

2.4 Built Form

As infill occurs it is important to ensure that the size of the building, the separation distance, the orientation of the building and the shape are considered

2.4.1 Building Floor Plates and Building Orientation

The visual impact of increased building heights and scale can be offset by limitations on floor plate sizes for taller buildings.

Constraints on floor plate size for the upper levels of buildings help to avoid bulky taller buildings and contribute to a more graceful skyline.

Floor plate size limits are not applied to the lower level of tall buildings in order to allow for greater design flexibility and to encourage continuous street wall conditions at the pedestrian level.

The size, articulation and orientation of a floor plate in a tall building is instrumental in the perception of the overall massing and visual impact of a building. The size and articulation of the floor plate of buildings over 6 storeys is key to maintaining shadow impact, loss of sky views and a potential of a wall effect from certain angles.

The use of smaller floor plates is required as they result in smaller shadows that tend to move quickly, improve sky

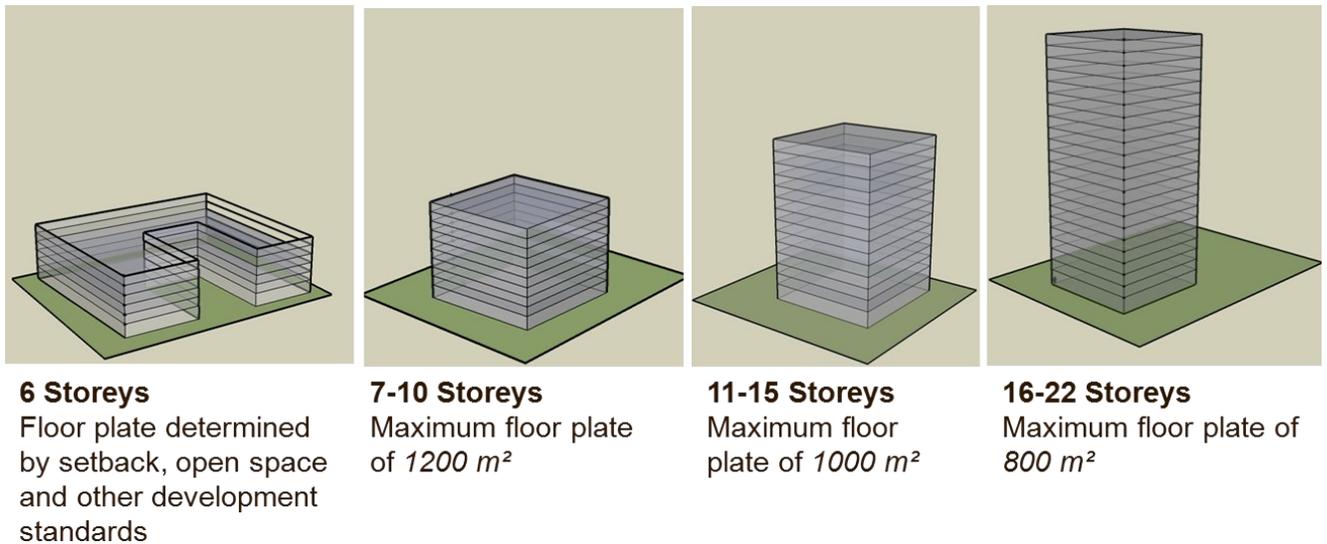


Figure B39 — Floor plate sizes for varying heights



Figure B40 — Go Transit Lands parking lot looking south. Floor plate of the Northshore building is 964 m² (as defined under GFA)

views and permit better views between buildings.

In addition, smaller floor plates promote sustainability by providing opportunity for increased daylight catchments and more efficient climate control within the building.

Buildings over 6 storeys should be designed so they are as square as possible to ensure minimal shadow impact and to ensure they do not create the visual impact of larger bulky floor plates. The maximum length of any building over 6 storeys should be 35 m including balconies for buildings under 15 storeys and 30 m for buildings over 16 storeys.

Currently the Residential Floor plates in the area range from approximately 550 m² to 1 350 m².

The maximum floor plates of buildings over 6 storeys, inclusive of balconies, shall be:

7-10 Storeys:

- Maximum floor plate of 1 200 m²

11-15 Storeys:

- Maximum floor plate of 1 000 m²

16-22 Storeys:

- Maximum floor plate of 800 m²



Figure B41 — Existing building in the Central Residential Precinct. Floor plate of 683 m²

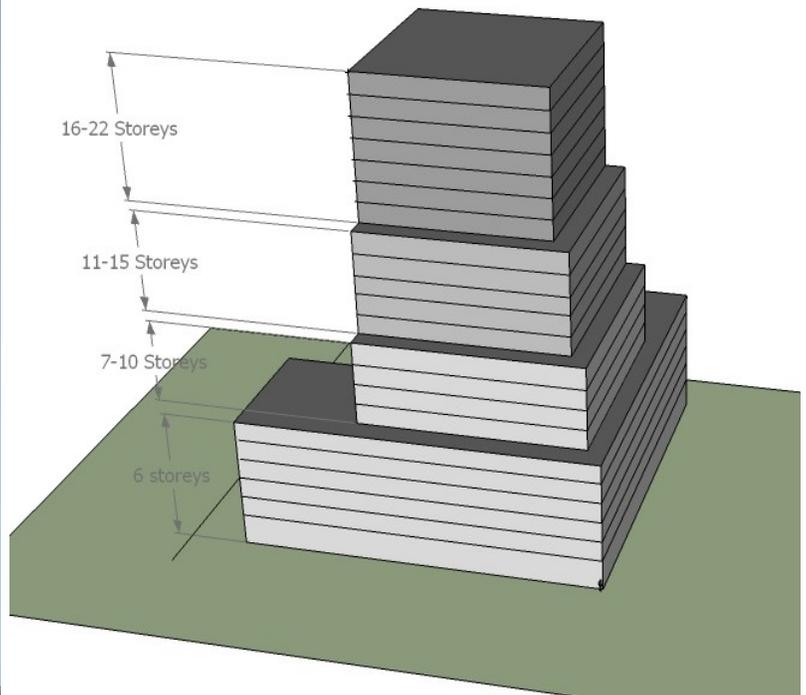


Figure B42 — Combination of building floor plates and heights may be permitted subject to other development criteria being met

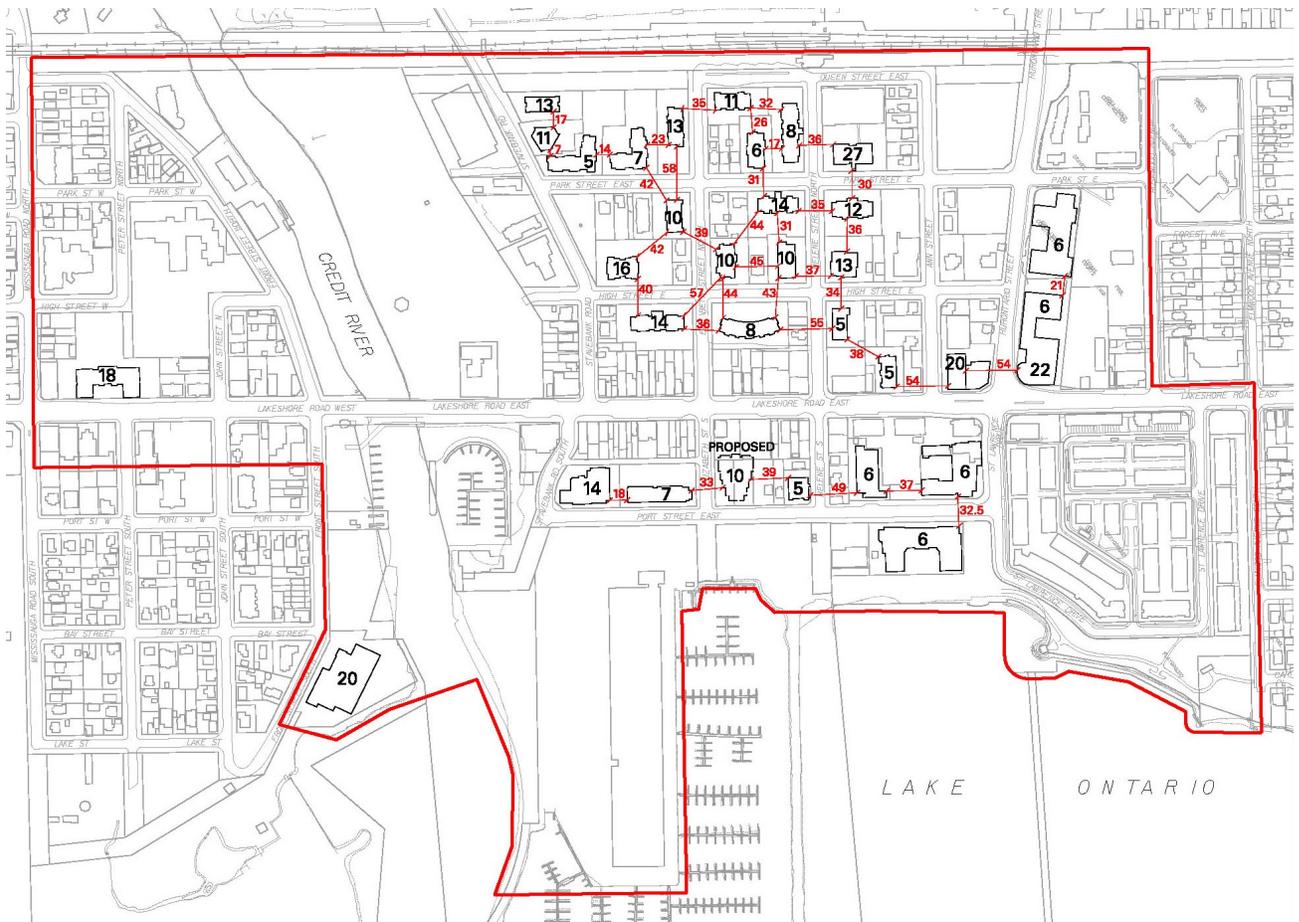
2.4.2 Building Separation Distances

The spacing between the faces of tall buildings enhances privacy, opens up views between buildings and permits access to sunlight and views of the sky.

Building separations in the Node are on average approximately 38 m for any building over 5 storeys, however many of the taller buildings are over 40 m apart.

A minimum of 35 to 40 m from any portion of a building that

is over 6 storeys to another building that is over 6 storeys is required. Taller buildings require greater separation distances and therefore will be required to meet the 40 m separation distance. These separation distances will ensure that new tall buildings maintain sky views, and develop as an elegant skyline. The current eclectic mix of low and high rise buildings, a diverse housing stock and mature landscaping should be maintained.



**PORT CREDIT NODE
BUILDING SEPARATION DISTANCES**

— NODE BOUNDARY



Produced by
T&W, Geomatics

Note: Separation distances are for buildings 5 storeys and greater in height.

Note: Separation distances are measured in metres.



October 2011

Figure B43 — Existing building separations

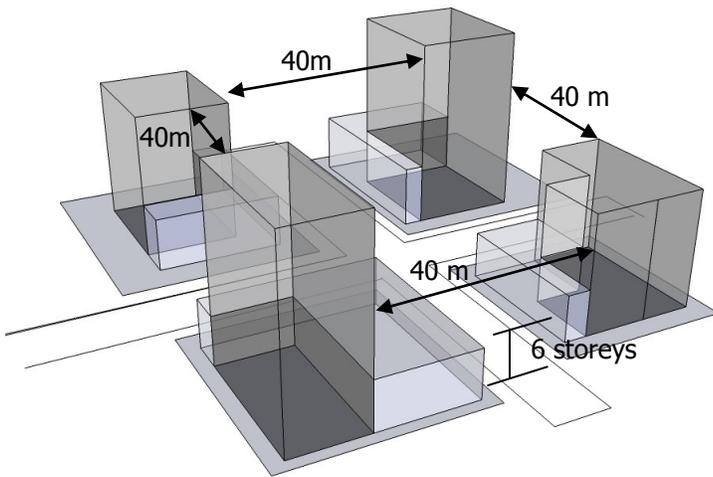


Figure B44 — Block to block separation distance

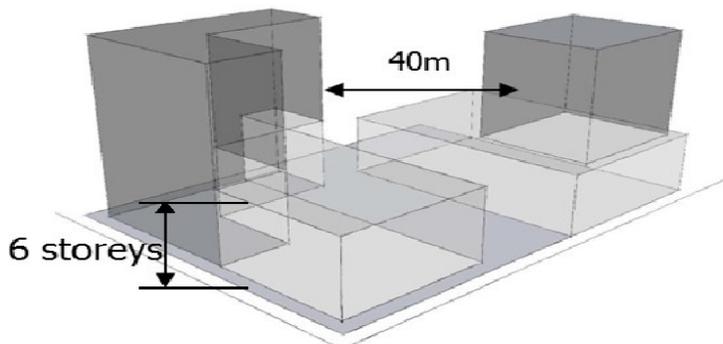


Figure B45 — Within a block separation distance



Figure B46 — Large existing separation distances

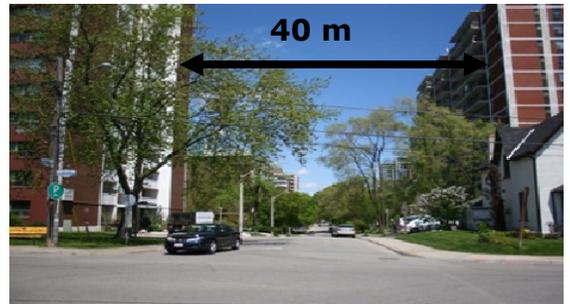


Figure B47 — Building separation distances on High Street East



Figure B48 — Building separation distances on Hurontario Street



Figure B49 — Building separation distances on High Street East

2.4.3 Skyline

A skyline is the overall or partial view of a city's tall buildings and structures consisting of skyscrapers in front of the sky in the background. It can also be described as the artificial horizon that a city's overall structure creates. Skylines identify the city or place from a distance. Taller buildings are typically located where there is a desire for visibility and way finding.

The skyline is the finger print of Port Credit. Key sites should be highlighted in the skyline to ensure way finding. The architecture of the top of buildings should be unique and should highlight the skyline and the location of the building.

It is important to maintain the existing views to Lake Ontario and within Port Credit and ensure that future buildings maintain sky views.

The placement and orientation of new buildings should also be oriented to maximize sky views along the length of Lakeshore Road East and the West Corridor so as not to create a wall effect.

New developments will be required to demonstrate how their building fits into the context through photographic imagery.



Figure B50 — View from Lakeshore Road West looking East



Figure B51 — View from Lakeshore Road East looking West



Figure B52 — View from Lake Ontario looking North at Port Credit

2.4.4 Site Size

The relationship of a site size to the size of the building and configuration should be considered in order to avoid a building overwhelming its site. In such cases, lower densities should be used. The form of buildings as a group should take precedence over the form of single buildings by virtue of considering the overall composition of the group.

Tall buildings must be set back a minimum of 10 m from side and rear property lines or the centre line of an abutting lane, measured from an external wall or exterior face of balconies to ensure maximum opportunity for fenestration and to ensure appropriate separation distances can be accomplished.

Sites that are too small to permit a tower with the required setbacks on all sides are not appropriate for tall buildings. Small sites are generally considered to be 45 m by 45 m for mid block and 40 m by 45 m for corner lots.

A building on a small site may only be constructed to 6 storeys above which a 45 degree angular plane may be used for additional levels set back from the street and the side and rear property lines.



Figure B53 — Maintaining sky views are essential in Port Credit



Figure B54 — Smaller sites should only construct smaller buildings

2.4.5 Microclimate

Tall buildings over 10.7 m can adversely effect the environmental quality of surrounding areas through the diversion of high speed winds and through the overshadowing of the adjacent public realm and residential buildings, including public/private amenity spaces.

The impact of both of these elements can be mitigated through good design and sensitive siting. The impact of shadows at different times of the day and throughout the year will need to be assessed. The use of architectural devices such as screens, terraces, awnings and also façade setbacks can be adopted to minimize the effects of high speed wind at the base of buildings and of shadow effects.

Individual proposals should seek to create well oriented and lively spaces that contribute positively to the wider public realm

2.4.5.1 Shadow Impacts

Shadow studies will be requested in support of development applications to demonstrate that the height and/or location of a proposed building will not generate excessive shadows over adjacent lands or the public realm.

Shadow studies will be required for buildings greater than 10.7 m in height which may cause a new shadow impact on adjacent residential properties, properties identified as listed or designated on the Heritage register, and public parkland, open space and the public realm. Particular attention will be focused on Lakeshore Road East and West and Hurontario Street where a comfortable pedestrian environment is strongly encouraged.

Development applications are to adhere to the City's Standards for Shadow Studies.

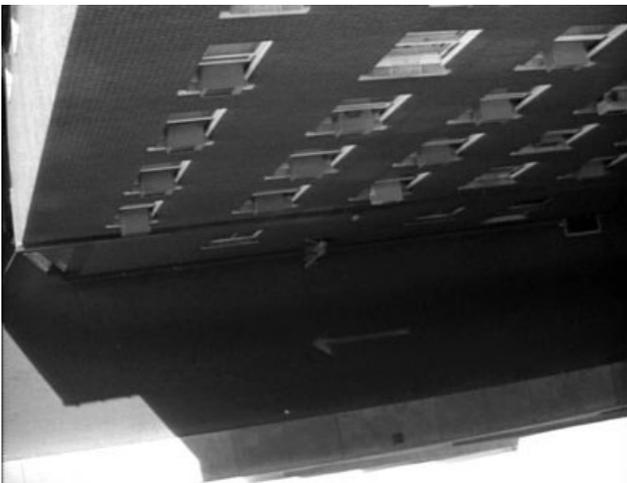


Figure B55 — Shadow on the public realm

The following dates and times will be required for shadow studies:

- March/September 21st from 9:12 a.m. to 6:12 p.m. on an hourly basis
- June 21st—8:12 a.m. to 8:12 p.m. on an hourly basis
- December 21st 9:12 a.m. to 4:12 p.m. on an hourly basis

2.4.5.2 Wind Comfort

The objective of the study is to ensure appropriate comfort and safety levels are maintained in the pedestrian realm, streetscapes, public spaces and areas immediately adjacent to and/or surrounding the proposed development.

The study may be required on development applications higher than 3 storeys or 16 m. The criteria to be used for the analysis should be signed and sealed by a certified engineer. The *Terms of Reference for Pedestrian Wind Comfort Studies* should be used.

The evaluation of the existing wind conditions in the immediate and surrounding area, prior to the proposed development will be required along with a comparison of the wind conditions based on the proposed development.

The evaluation will include conditions based on seasonal variations (i.e. summer, spring, fall and winter). The intended use of the area will be considered to determine the appropriate wind conditions that will be permitted.

Areas will be considered appropriate for their intended use if microclimate/wind conditions are satisfied 80%.

Categories will be classified by their intended use and appropriate impact:

- i) **Sitting - 0 to 10 km/h:** reading and seating areas, outdoor cafés.
- ii) **Standing - 0 to 14 km/h:** passive areas, building entrances, short term seating.
- iii) **Walking - 0 to 19 km/h:** areas with pedestrian movement, sidewalks, street frontages.
- iv) **Uncomfortable - greater than 19 km/h:** areas with little pedestrian activity.

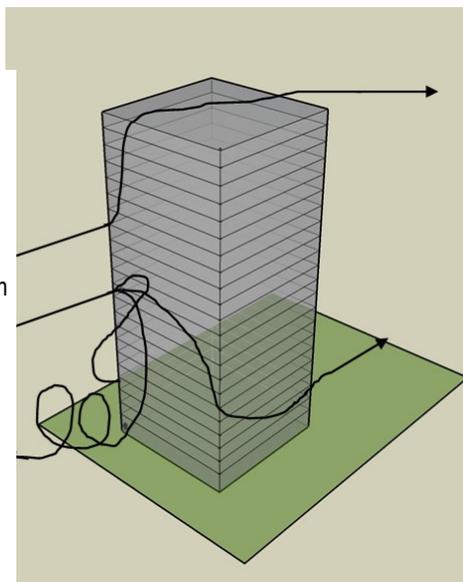


Figure B56 — Potential wind conditions on a tall building without a podium

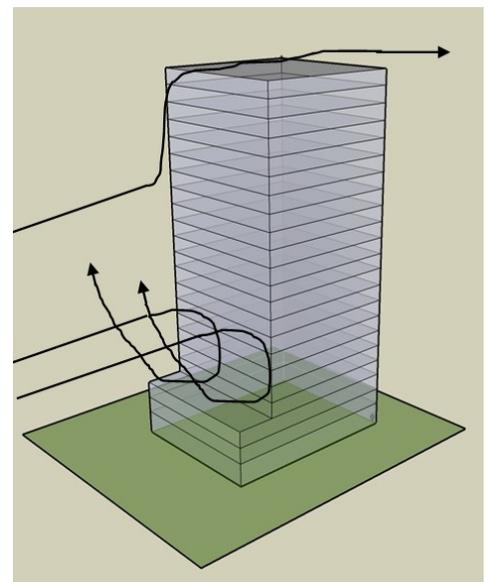


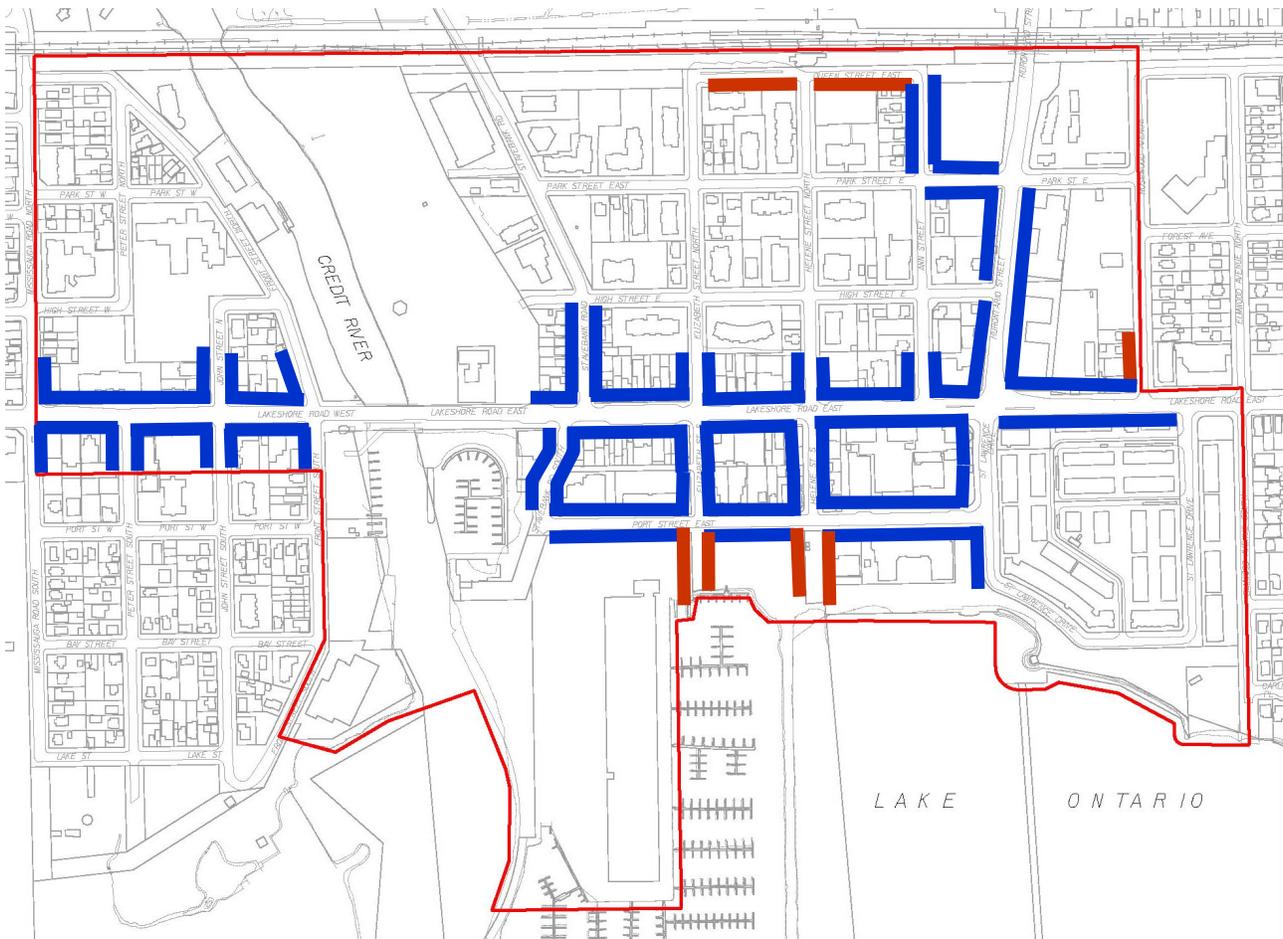
Figure B57 — Potential Wind conditions on a tall building with a podium

2.4.6 At Grade Commercial Requirements

Ground floor retail uses concentrated at important areas within the Node are vital to the function of Port Credit. Retail primarily exists along Lakeshore Road East and West with some smaller scale retail extending on side streets off of Lakeshore Road East and West. Retail along Hurontario Street is also an important continuation of the mixed use area.

Commercial uses will be required along Lakeshore Road East/West; along Hurontario Street; in proximity to the GO Transit Station where it is an essential component of transit oriented development; along Port Street; and along the routes that connect them as indicated in Figure B58 as areas outlined in blue.

Areas outlined in red are streets where retail is encouraged but not required and are considered transitional streets.



PORT CREDIT NODE
POTENTIAL MAXIMUM HEIGHT (STOREYS) RESTRICTIONS

DRAFT



Produced by
 T&W, Geomatics

— NODE BOUNDARY

Note: lines represent general approximation and could be adjusted subject to study



October 2011

Figure B58 — At grade uses in the Port Credit Node

Retail Guidelines:

- a. A main front usable door shall face Lakeshore Road East/West;
- b. Generally retail areas require a minimum of 4.5 m (15 ft.) of clear height from grade and a minimum of 15 m (50 ft.) of frontage;
- c. A minimum of 75% glazing is required for retail storefronts along the street wall;
- d. A minimum 6 m (20 ft.) store front extension around the corner from a primary street where retail is required;
- e. Retail tenant signs shall be designed of high quality material, colour and scale in keeping with the design of the building;
- f. Fascia signs shall be limited to the first floor level;
- g. Tenant signage shall be of a consistent design if there is more than one tenant;
- h. Ground signs are prohibited;
- i. Store front window signage is permitted up to 25% of the glass surface area and shall not block clear views of exits or entrances and shall maintain visibility into the interior of the premises at all times; and,
- j. Tables and other active uses adjacent to storefront windows are encouraged where permitted.

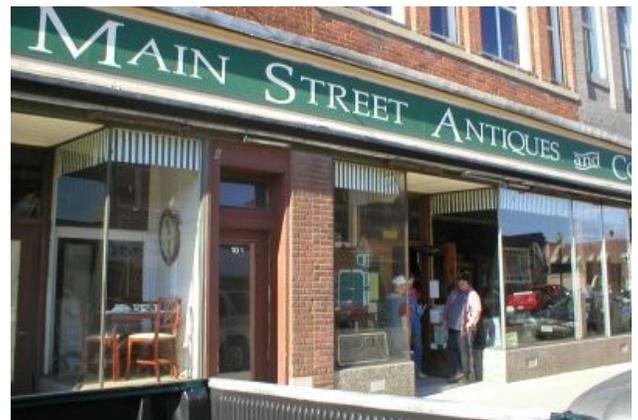


Figure B59 – Image examples of Mainstreet conditions

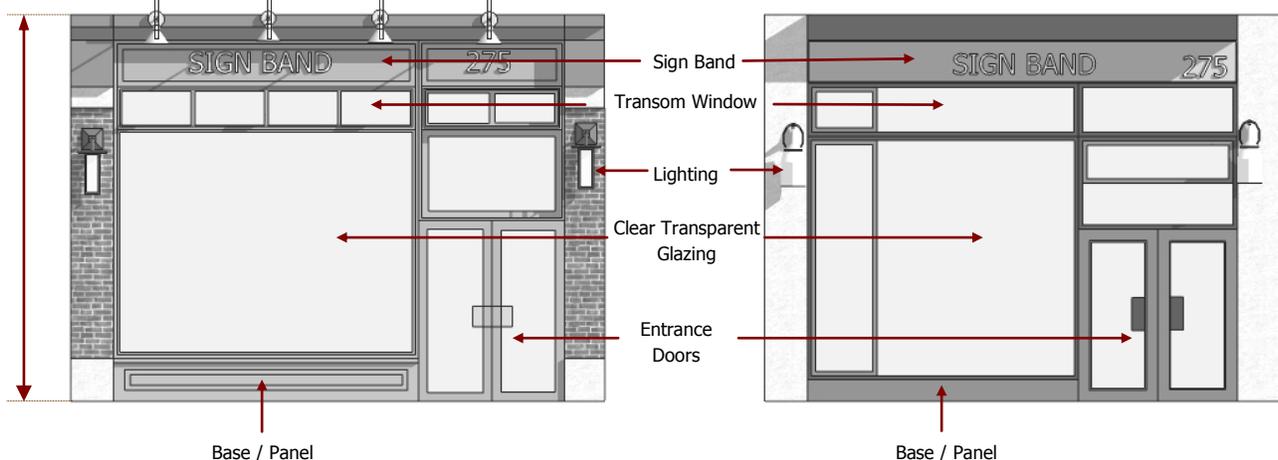


Figure B60 – Illustration of Mainstreet retail

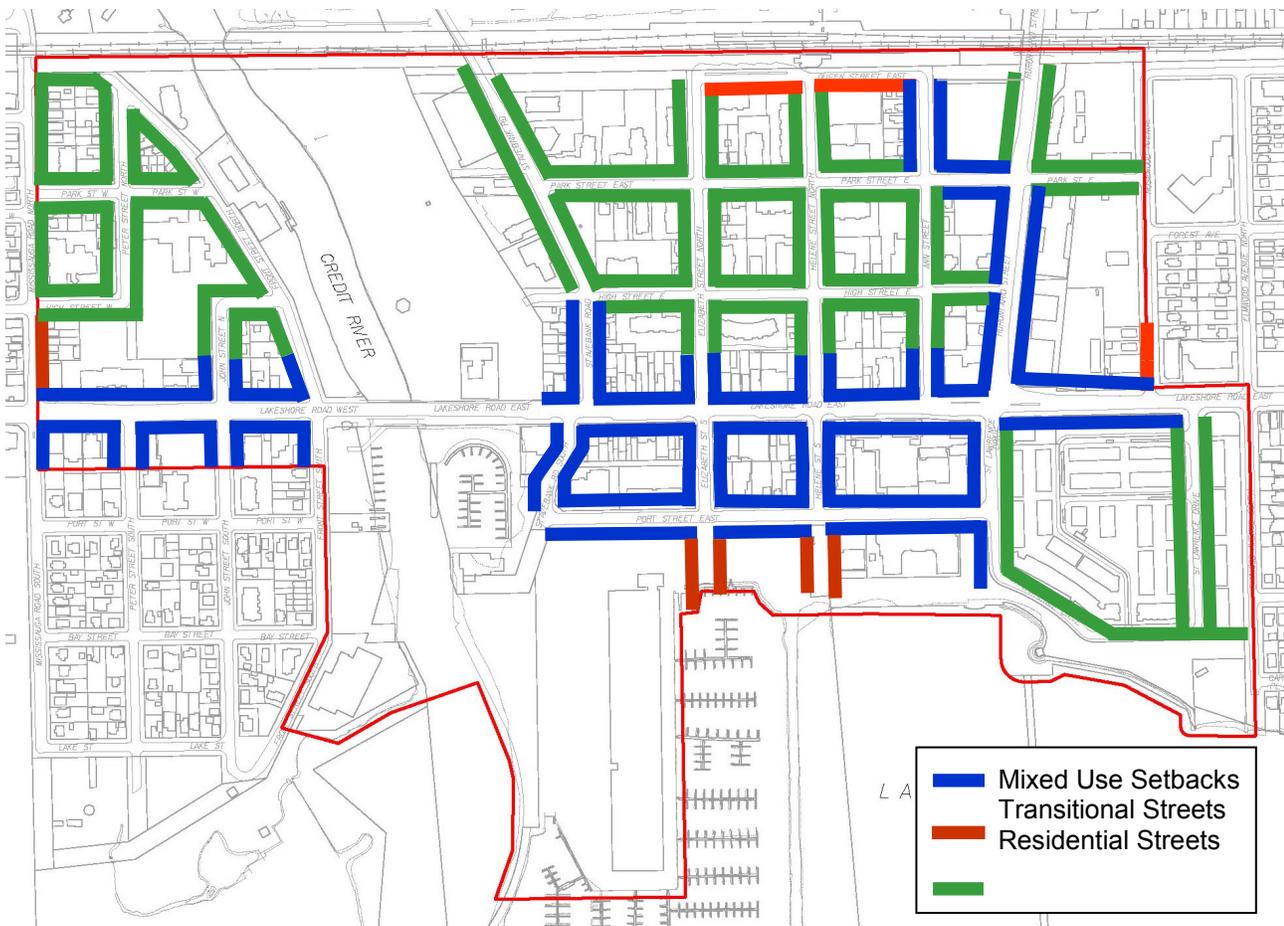
2.4.7 Building Setback

Mixed Use Setbacks

Setbacks on streets where retail is required should generally be 0.0_m to 3.0 m from the property line. The exact location of new buildings will be determined once the dimension of the public realm has been achieved. New development must ensure that a minimum of 5.6 m public realm from the sidewalk/street curb to the face of the building can be accommodated to ensure appropriate streetscape treatment can be achieved.

Transitional Streets

On transitional streets that are shown as red on Figure B61, commercial uses will be encouraged however not required. Buildings should be designed so that they may be converted to commercial uses when market conditions allow.



**PORT CREDIT NODE
Building Setbacks**

DRAFT



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Note: lines represent general approximation and could be adjusted subject to study



October 2011

Figure B61 – Building Setback Requirements

Residential Street Setbacks

On residential streets (shown on Figure B65 as green) the setback to a building shall be a minimum of 4.5 m and a maximum of 7.0 m depending on the character of the adjacent developments and the configuration of the proposed building. The setback should ensure that there is ample appropriate landscape treatment to fit in with the existing character of the community.

Sidewalk curb to the face of the building



Figure B64 — Sidewalk curb to face of the building for new developments will be 5.6 m



Figure B62 — Mainstreet setback of 0.6 m to 3.0 m. Setback varies depending on the size of the public realm



Figure B65 — Residential setbacks should be well landscaped. Setbacks should be a minimum of 4.5 m and a maximum of 7.0 m



Figure B63 — Residential setbacks should be well landscaped. Setbacks should be a minimum of 4.5 m and a maximum of 7.0 m



Figure B66 — Transitional areas such as Helene Street South

2.4.8 Building Frontages

Mixed Use Streets

Port Credit consists of closely spaced mainstreet buildings with no interruptions in the sidewalk. This ensures a safe pedestrian environment.

Buildings fronting onto streets that are required to have retail should be closely spaced with no driveway access points. Front doors shall face Lakeshore Road

A minimum of 90% of the building face shall front onto Lakeshore Road and be within 0.6 m to 3.0 m of the front property line.



Figure B67 — Mainstreet building frontages



Figure B68 — 90% of the building frontage is 0.6 m to 3.0 m

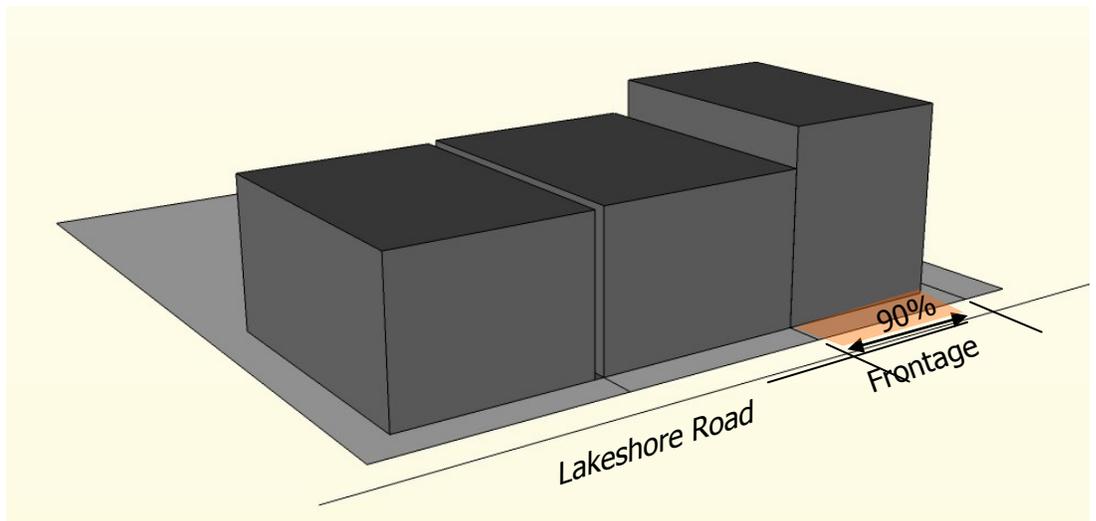


Figure B69 — A minimum of 90% of the building shall front onto Lakeshore Road East and West and be within 0.0 m to 3.0 m

Transitional Streets

Buildings fronting onto transitional streets should be closely spaced with minimal driveway access points. Front doors shall face the street. A minimum of 70% of the building face shall front onto the street and be within 0.0 m — 3.0 m of the front property line.

If residential units are proposed at grade, the building should be set back 4.5 m to 7.5 m.



Figure B70 — 70% of the frontages along Port Street East shall be 0.6 m to 3.0 m if no residential uses are proposed



Figure B71 — 70% of the building frontage should be 4.5 m to 7.5 m if residential units are proposed

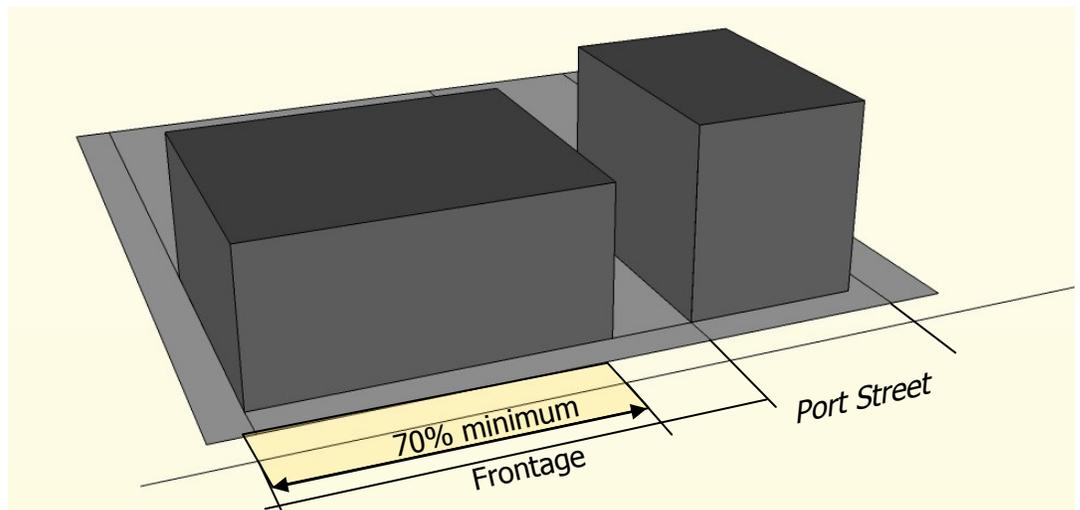


Figure B72 — A minimum of 70% of the building shall front onto Port Street East and be within 0.6 m to 3.0 m

2.4.9 Landscape Area

The Port Credit Node precincts, particularly the Central Residential Precinct and the Riverside Precinct are characterized by well landscaped front yards and mature trees. The landscape area is defined as any outdoor area on a lot, located at grade, including the landscaped buffer area. It is suitable for the growth and maintenance of grass, flowers, shrubs, trees and other landscape features, and may include walkways, berms, retaining walls and outdoor amenity areas.

However, it shall not include driveways, aisles, ramps or internal roads, parking areas whether surfaced or not, curbs, any open space beneath or within any building, structure or

part thereof, or any exterior garbage storage or handling area.

A minimum of 30 % landscape area is required for all sites within the Central Residential Precinct, the Riverside Precinct and the Harbour Mixed Use Precinct. This is to ensure that all lots can achieve a minimum buffer to adjacent uses; that the existing context which contains well landscaped front yards, particularly on high density sites, can be achieved; to ensure sustainable measures can be attained so fenestration can be achieved; and that overdevelopment of sites does not occur. It will also help protect views to Lake Ontario through future development of sites along the waterfront.

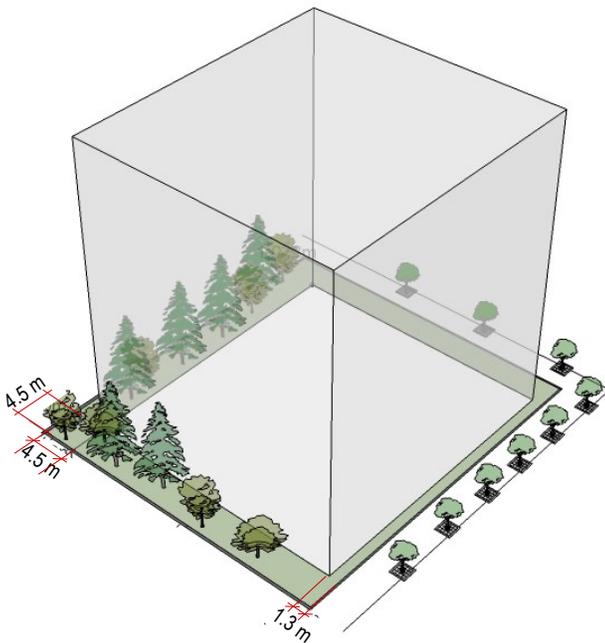


Figure B73 — 30% landscape area for a small lot on a 40 m x 45 m lot size produces the minimum landscape buffer requirement of 4.5 m on the rear and side lot line. It does not maintain the required front and exterior side yard back from the front property line.



Figure B74 — Image of the Riverside Precinct



Figure B75 — Image of the Central Residential Precinct

At a minimum, the landscape area should achieve a row of coniferous trees with shrubs and/or perennial plantings to provide a natural visual buffer between the two uses.

In the Mainstreet Precinct, where development is intended to create a compact commercial mainstreet, no minimum landscape area will be required, however, a minimum 4.5 m landscape buffer is required when a mixed use zone abuts a residential zone.

A landscape buffer is defined as a continuous, open, unobstructed width of land substantially parallel to and adjoining a lot line that is intended for the growth and maintenance of plant material including trees, shrubs and other landscape features such as retaining walls.



Figure B76 — Image of the Mainstreet Corridor Precinct



Figure B77 — Image of the Harbour Mixed Use Precinct



Figure B78 — Image of the Central Residential Precinct



Figure B79 — Image of the Harbour Mixed Use Precinct

2.4.10 Pedestrian Realm/Streetscape

The public realm is an integral part of any site development. The relationship between the buildings, site layout and elements within the public realm has a great impact on the urban form and the experience of those who live, work and play in Port Credit.

New developments should enhance public streets and the open space system by creating a desirable street edge

condition that is ideal for the use of pedestrians.

Different streets will have different streetscape requirements depending on their uses. Sidewalk width and content should relate to its function. For example, streets that include public transit should have wider sidewalks and room for street furniture related to the bus stops.

Parking garage ventilation should not be located at grade along any street frontages and should be integrated into the façade of the building.

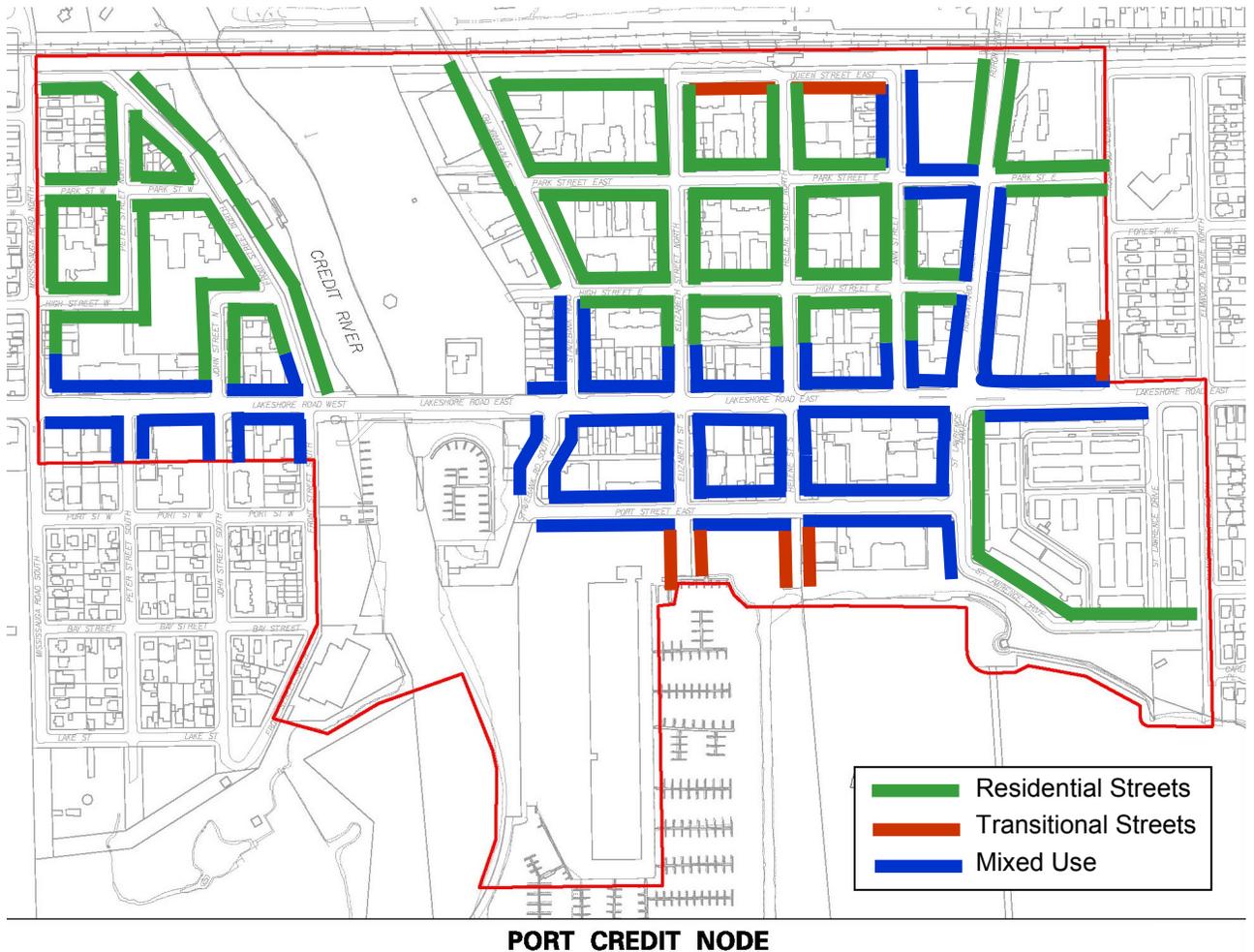


Figure B80 — Image of the pedestrian realm/streetscape

Mixed Use

The vitality of the Node depends on the health of the mixed use and its local business. Lakeshore Road East and West require streetscape improvements which will help to revitalize the most important streets in Port Credit.

Additional requirements for building setbacks may be required to achieve the ideal pedestrian experience within the streetscape corridor.

A Master Streetscape Study is needed for Lakeshore Road

East and West, Hurontario Street, Port Street and the streets connecting these locations.

Utilities such as overhead wires and underground cables are important uses that occur within the road right of way. Careful consideration will be given when planning streetscape improvements such as tree planting, to ensure compatibility with utilities. The design of utility infrastructure should reflect best practices in design, while ensuring sufficient technical resilience to provide for increasing number and quality of service demanded by the public.



Figure B81 — Conceptual mainstreet streetscape



Figure B82 — Conceptual mainstreet streetscape



Figure B83 — Lakeshore Road West older streetscape treatment



Figure B84 — Lakeshore Road East recent development and streetscape treatment

Residential Streets

Residential streets are those that have residential uses at grade. Residential streets are characterized by generous setbacks, upgraded tree planting and landscape treatment.

Residential streets should continue to maintain these characteristics. The pedestrian realm will include a sidewalk on the street edge where appropriate with tree planting and grassed areas on the interior of the sidewalk.

New development will ensure that existing trees are preserved, maintained and enhanced.

Buildings will generally be set back 4.5 m to 7.0 m to ensure an adequate landscape treatment can be accommodated at grade.



Figure B85 — *Hurontario Street South East side*



Figure B86 — *Riverside Precinct*



Figure B87 — *Riverside Precinct*



Figure B88 — *High Street looking West*

Transitional Streets

Transitional streets are those that may one day be used as commercial, retail or employment streets as Port Credit evolves into a more compact village.

These areas should be developed with flexibility. Buildings should be located within the setback of 0.0 m to 3.0 m for commercial retail and 4.5 m to 7.0 m for at grade residential uses. The pedestrian realm and streetscape should be continuous and provide a wider pedestrian realm to ensure

flexibility. Street trees and coordinated furniture including post and ring bike parking are a requirement.



Figure B89 — Port Street East



Figure B90 — Ann Street



Figure B91 — Port Street East



Figure B92 — Hurontario Street

2.4.11 Parking, Loading and Service Areas

The design of parking, servicing and loading areas are a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens less desirable aspects and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians, cyclists and vehicles.

Service, loading and garbage storage areas should be integrated into the buildings, located interior to the building or alternatively at the rear of the building and screened from the public realm and adjacent residential uses. Parking should be located underground, internal to the building or to the rear of buildings.

Above grade parking facilities will be designed to be compatible with the surrounding character through the use of architectural elements that fit with the scale, style and streetscape of the community. Consideration should be given to active pedestrian related uses on the ground floor of the structure in order to improve the animation of street edge conditions. Above grade parking structures should be designed in such a manner that they are integrated into the development. Vehicles should not be visible to the public realm.



Figure B93 — Laneway south of Lakeshore Road East



Figure B94 — Laneway south of Lakeshore Road East



Figure B95 — Laneway north of Lakeshore Road East

2.4.12 Roof Top Mechanical Penthouse Units

All rooftop units should be internal to the buildings and hidden from public view.

All mechanical penthouses should be designed and clad with materials to compliment the building façade.

The portion of the roof not utilized as mechanical penthouse should be developed as green roofs and/or useable outdoor amenity space.



Figure B96 — Green Roofs on buildings



Figure B97 — Example of roof top mechanical that has been integrated and designed into the building



Figure B98 —Green Roof Example

2.4.13 Architectural Expression and Materiality

New buildings should reference their physical, cultural and historic surroundings through their architectural language and high quality materials. Materials should show sensitivity to their surroundings and should aim to be of the highest quality, responding directly to the existing urban fabric, whether by utilizing similar or sympathetic materials or by positive contrast.

Materials should be chosen with regard to their performance in sustainable terms. The use of local or recycled materials, and/or materials from renewable resources is encouraged.

High quality materials will be required in all new developments. The material most widely found in Port Credit is red tone brick. New developments are encouraged to incorporate this material into their development.

For the mainstreet, vision glass should be used for all store fronts. No spandrel glass will be permitted along Lakeshore Road.

Materials that are not dominant in Port Credit are discouraged as the dominant feature of any new development. These materials include architectural concrete block, stucco, spandrel glass and EIFS panels. These materials will not be permitted below the 4th storey. Concrete block is not permitted to be exposed.

Building scale should be broken down through the use of stepping, projections, canopies, trellises, changes in scale, fenestration patterns, materials and finishes.

The private space that extends from the building face to the public right-of-way must be designed in a such a way that it seamlessly blends with the design of the public realm.

Future sidewalk/boulevard treatment including site elements such as seating and lighting, should match that of the public right-of-way in order to blur the line between public and private realm.



Figure B99 — Preferred materials to be exposed: brown/red brick



Figure B100 — Vision Glass/Brick Combination



Figure 101 — Balconies should be designed so that they are part of the building rather than added onto the building as an afterthought. Glass balconies should have 50% tinting to ensure items on the balcony are not fully exposed



Figure B102 — Prohibited to be exposed: Concrete Block

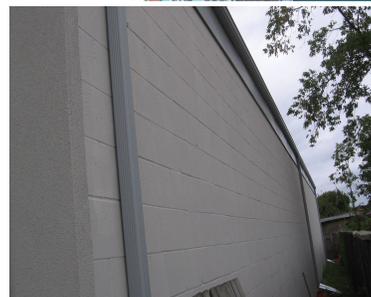


Figure B103 — Prohibited to be exposed: Painted Concrete Block



Figure B104 — Discouraged materials: Architectural Concrete Block



Figure B105 — Discouraged materials: Stucco (synthetic or natural)



Figure B106 — EIFS Panels for more than 20% of the building material and should not be placed within the first 2 storeys



Figure B107— Glass balconies should be 50% tinted



2.5 Scenic Routes, Key Sites and Views

Currently, the Port Credit community has physical and visual access to the Lake Ontario and the Credit River. As development occurs, views to the water should be preserved and enhanced as they are major contributors to the community's character and pride.

Public views of important natural or man-made features along streets and scenic routes need to be protected as

they add value to the built form and contribute to neighbourhood identity.

When new development occurs, it must maintain, and in some cases, enhance these views and vistas to prominent features.

Sites with prominence, high visibility and access should be considered as a priority for civic buildings and community

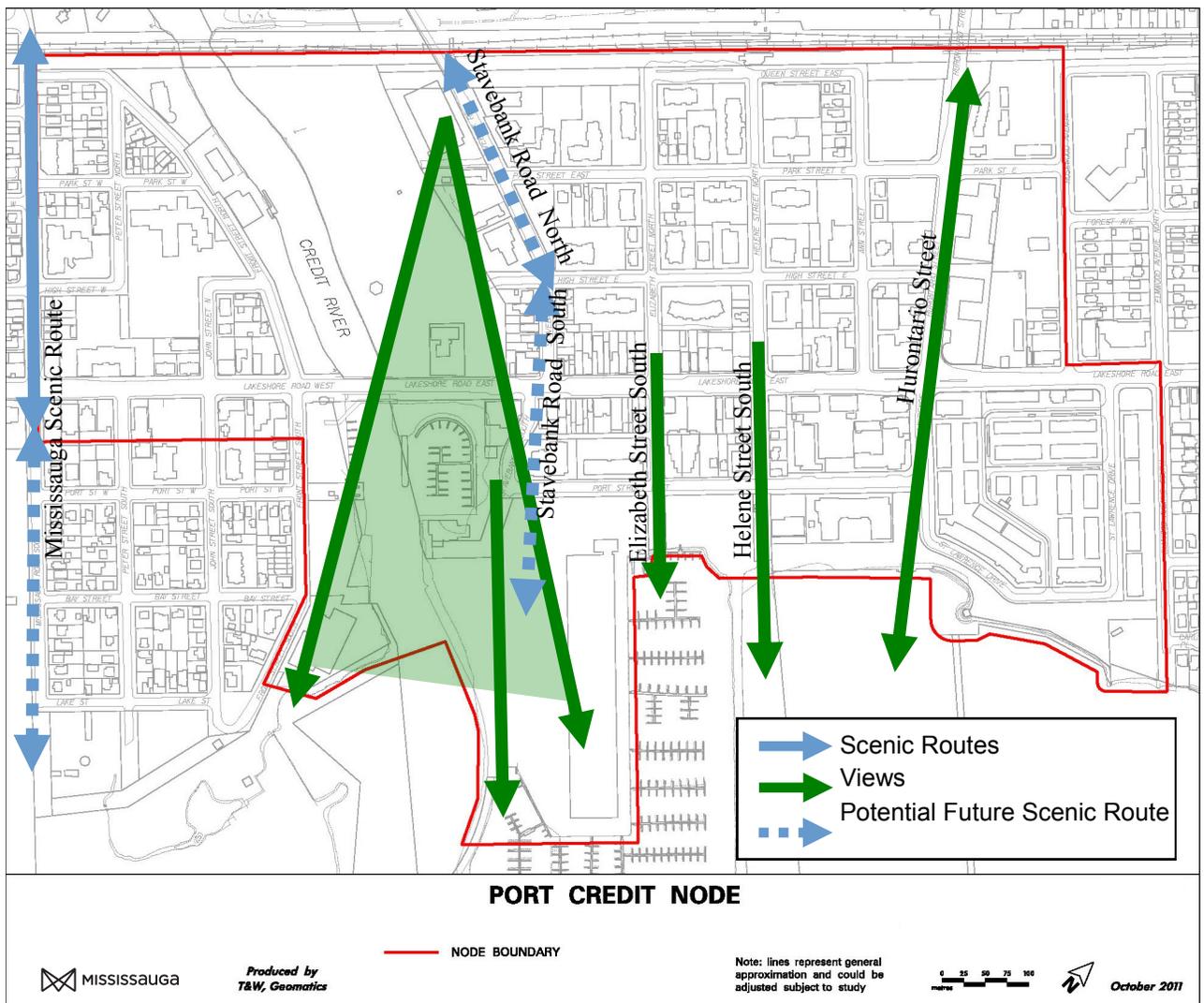


Figure B108 — Scenic routes and views

infrastructure.

Special care will be taken with development along Scenic Routes to preserve and complement the scenic historic character of the street. Scenic Routes and views identified in Figure B108 should be preserved and enhanced.

Mississauga Road Scenic Route



Lands abutting the Mississauga Road right-of-way between the St. Lawrence and Hudson Railway and Lakeshore Road West (frontage, flanked and rear yards) are designated a scenic route and will be subject to special policies as outlined the Mississauga Official Plan.

Potential Future Scenic Routes



Stavebank Road North and South from the Railway tracks to the Port Credit Pier and Mississauga Road South have been identified as potential future Scenic routes that will be required to be studied further through separate review.

Views



Port Credit has many natural views to Lake Ontario. These views shall be enhanced and maintained. There are a number of views that still need to be developed. These include the extension of Elizabeth Street South and Helene Street South. Streetscapes along these streets should enhance the views to Lake Ontario. Architectural interest should be heightened on these corridors.

In addition to the views to Lake Ontario Port Credit is visually connected to Downtown Mississauga.

Views to prominent Architecture “The Marilyn” in the Absolute project should be maintained and enhanced along Hurontario Street.



Figure B109 — Mississauga Road Scenic Route



Figure B110 — Hurontario Street looking North, “Marilyn Building” in the background



Figure B111 — View from Lake Ontario

2.6 Place Making Opportunities

Place-making is the process that fosters the creation of vital public destinations: the kind of places where people feel a strong stake in their communities and a commitment to making things better.

Place-making capitalizes on a local community's assets, inspiration, and potential, ultimately creating good public

spaces that promote people's social interaction, health, happiness, and well being.

Although the entire Node, as a centre for surrounding neighbourhoods, should be recognized as offering place-making opportunities, the following represent areas which have the opportunity to make a substantial contribution to Port Credit:

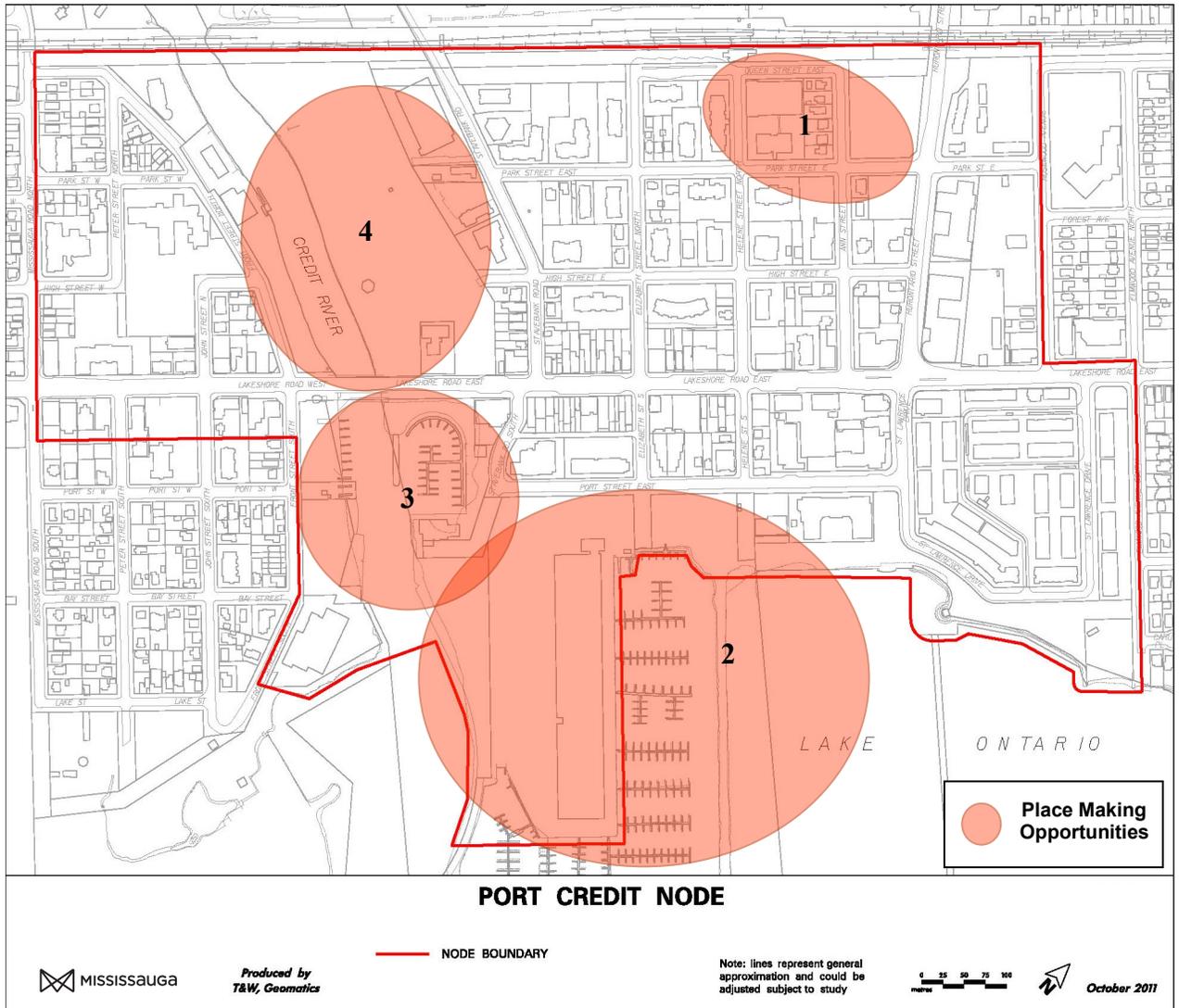


Figure B112 — Place-making opportunities

1. Transportation hub: the vicinity of the GO station, parking lot, and future LRT;
 2. Waterfront marina: the vicinity of the Port Credit Harbour Marina, future LRT stop on Port Street East, and terminuses of Stavebank Road South, Elizabeth Street South, and Helene Street South;
 3. Mouth of the Credit River: the vicinity of Snug Harbour, J.J. Plaus Park, Marina Park, Heritage Building (former Canada Post Office), Region of Peel Lighthouse; and
 4. Credit River Valley: vicinity including Port Credit library, Port Credit Arena, Memorial Park, Canoe and Rowing Clubs, Legion Hall, Stavebank Road properties which back onto the valley.
- When reviewing development applications, consideration should be given to capitalizing on any opportunities that may foster place-making and would contribute to the urban form of Port Credit.
- The inclusion of Public Art should be considered in these locations.



Figure B113 — (1) Go Station parking area



Figure B114 — (1) Go Station parking area



Figure B115 — (2) Port Credit Harbour Marina



Figure B116 — (2) St Lawrence Square



Figure B117 — (2) St Lawrence Park Lookout



Figure B118 — (2) St Lawrence Park



Figure B119 — (3) J.J. Plaus Park



Figure B120 — (4) Port Credit Memorial Park



Figure B121 — (4) Credit River

2.7 Continuous Waterfront Access

The Lake Ontario shoreline is an integral component of the green system and is a key Provincial linkage due to the unique ecological functions and habitats it provides. In addition, it has an important role in leisure activity and tourism.

As a condition of development approval, provisions of signifi-

cant public parklands and access to the waterfront including the extension of the Waterfront Trail, will be required.

Continuous waterfront access along the shoreline is a priority for the residents of Port Credit and the City as a whole. In these situations an appropriate balance between use and public access must be found.

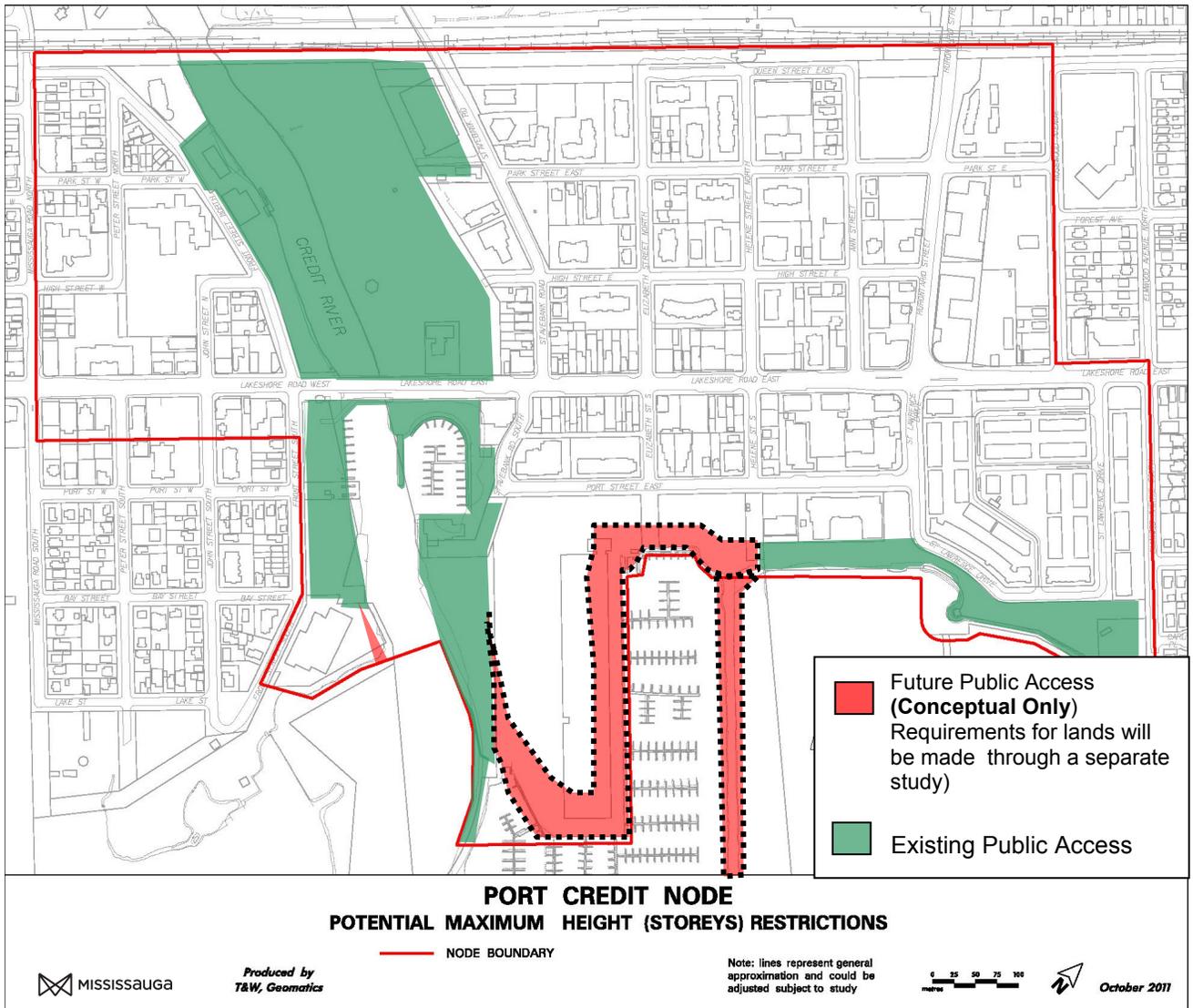


Figure B122 — Continuous Waterfront Access, Existing and proposed



Figure B123 — Port Credit Harbour Marina



Figure B124 — St Lawrence Park



Figure B125 — Credit River



Figure B126 — J.J. Plaus Park



Figure B127 — Credit River



Figure B128 — Port Credit Harbour Marina



Figure B129 — Port Credit Harbour Marina



Figure B130 — Boat launch, Marina Park



Figure B131 — St Lawrence Park



Figure B132 — St Lawrence Park

2.8 Cultural Heritage Resources

Heritage is an important characteristic of the Port Credit Node. Within the Port Credit Node there are over 45 properties that are on the City's Heritage Register. The Register contains two types of properties:

- Designated (recognized by the City through by-law as being of Heritage significance); and
- Listed (identified but not fully researched as to heritage significance).

In addition, the Port Credit Node also contains Cultural Landscapes which can be defined as a setting which has enhanced a community's vibrancy, aesthetic quality, distinctiveness and sense of history or sense of place. Cultural landscapes within the Port Credit Node include:

- Port Credit Harbour;
- Port Credit Pier;
- the CN Bridge over the Credit River;

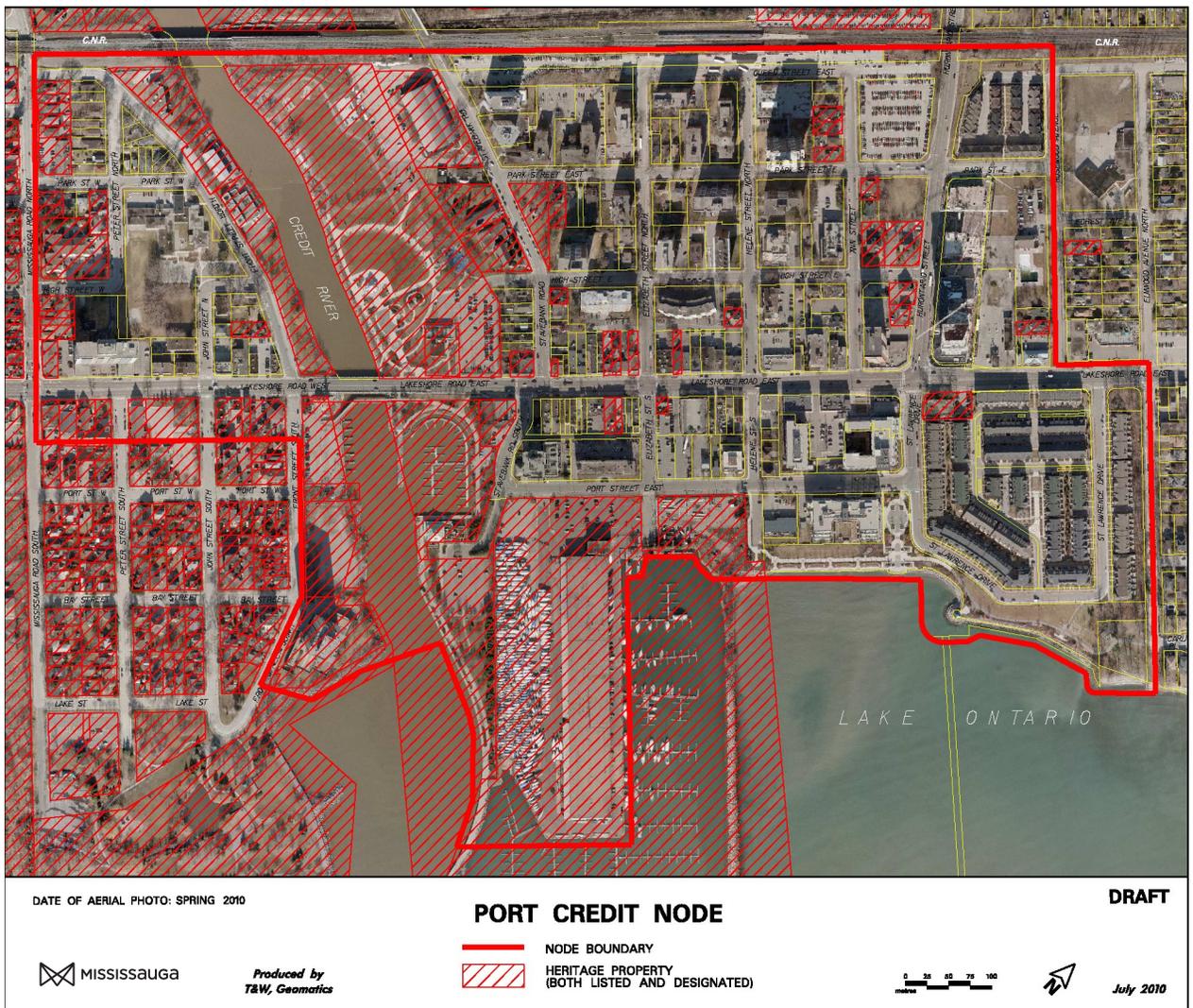


Figure B133 — Cultural Heritage Resources

- Credit River Corridor; and
- Mississauga Road Scenic Route.

The Port Credit Node has 10 properties that are designated. Designated properties are to be retained.

The retention and enhancement of heritage listed properties is strongly encouraged. Properties listed on the Heritage Register will be preserved in their existing location. Any development will incorporate these structures in the design of the proposal. Any changes to these structures or developments adjacent to these structures will require a Heritage Impact Statement and may have additional requirements. Additional requirements may include, but are not limited to, a review and approval from the Heritage Advisory Committee and Council.

Tall buildings will not visually impede the setting of listed/designated heritage buildings. Where heritage buildings are low-scale, the podium of the tall building will respect and reflect the unique urban character, visual relationships, topography and materials of the surrounding historic building. The tall buildings will preserve and enhance the character and appearance of the setting of the adjacent listed/designated building(s).

Where heritage buildings are low-scale, the podium or ground façade of tall buildings will respect and reflect the unique urban grain and scale, visual relationship, topography and materials of the surrounding historic buildings.

For lands on the south side of Lakeshore Road West, between Mississauga Road and the Credit River, the *Old Port Credit Village Heritage Conservation District Plan* applies.



Figure B134 — 90 High Street East, Designated



Figure B135 — 84 High Street East, CG Hamilton House, Designated



Figure B136 — 12 Peter Street South, Designated



Figure B137 — 141 Lakeshore Road West, Designated

3

Port Credit Neighbourhoods

Port Credit Neighbourhoods

Port Credit neighbourhoods are located on either side of the Port Credit Community Node. Lands identified as Neighbourhood Precincts are considered to be stable areas where the existing character is to be preserved and will not be the focus for intensification.

Although stable, some change is anticipated. New development does not necessarily have to mirror existing development types and densities, however it will respect the character of the area. New development does not necessarily have to mirror existing development types and densities, however, it will respect the character of the area.

The Port Credit Neighbourhood Character Area is comprised of five precincts:

- North Residential (which include the Shawnmarr/Indian Heights and Credit Grove neighbourhoods);
- South Residential (which includes the Cranberry Cove and Hiawatha neighbourhoods);
- Mainstreet Neighbourhood ;
- Historic Village of Port Credit; and
- Vacant Former Refinery.

The following sections provide additional context and information on each of the Neighbourhood Precincts. Subsequent sections provided further direction on specific built form issues.

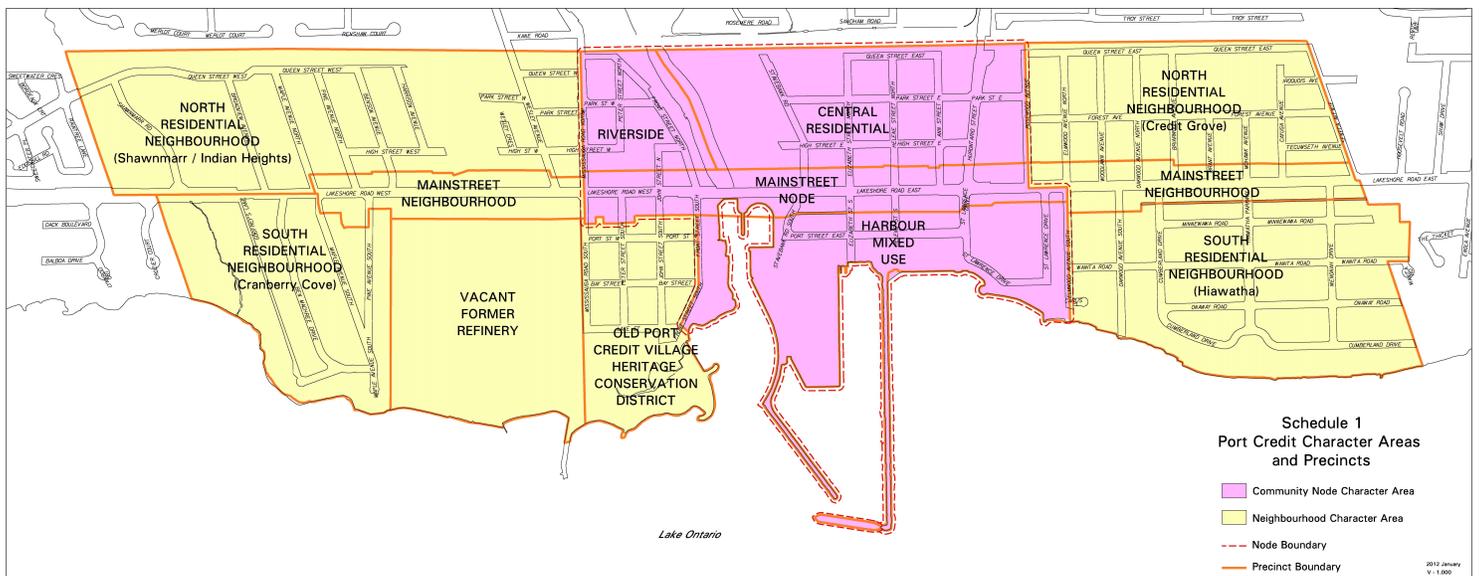


Figure C1 — Port Credit Neighbourhood Precinct Areas



Figure C2 — Port Credit Neighbourhood Areas



Figure C3 — Port Credit Neighbourhood Areas

3.1 Contextual Building Heights

Port Credit neighbourhood areas are characterized by predominately low rise buildings of 3 storeys or less. There are a limited number of older 4 to 6 storey apartment buildings located along Lakeshore Road or in the Heritage Precinct.

There is also an 8 storey apartment building representing the tallest structure in the Neighbourhood Precincts, located at Lakeshore Road West and Maple Avenue South.

While Neighbourhoods are to be stable, some change is anticipated. Where development occurs it will generally be through modest infilling or development within the mainstreet precinct, the existing commercial plaza or vacant former refinery site.

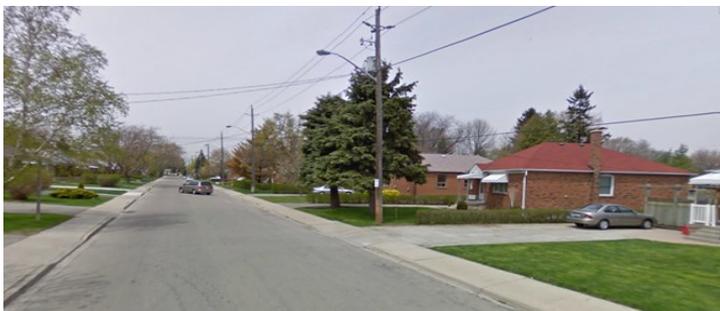


Figure C5 — Port Credit Neighbourhood Precinct Areas

3.2 Planned Building Heights

Building heights reflect the existing context and planned function for the Neighbourhood Character Area and are summarized in Figure C6. In some cases, building heights are reduced in the zoning by-law thereby requiring a proposal to go through a process to achieve the maximum height permitted.

Additional information is provided in the following sections on the individual precincts and subsequent discussions on specific built form issues.

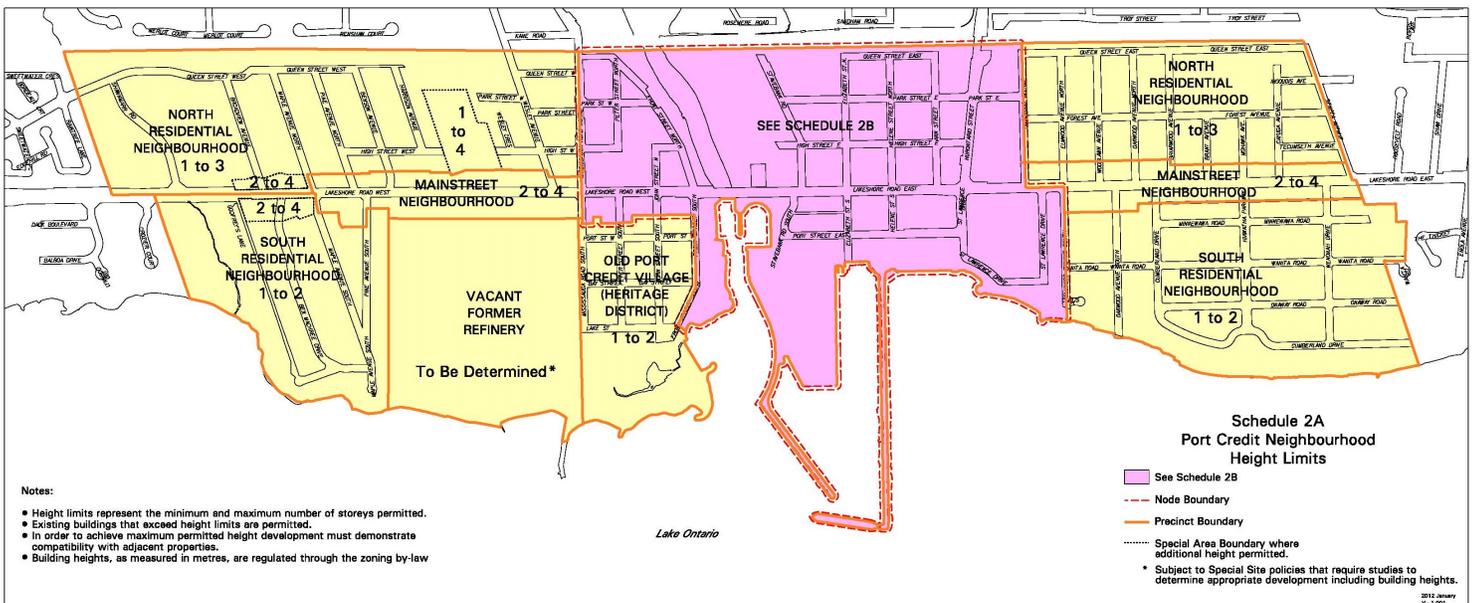


Figure C6 — Port Credit Neighbourhood Precinct Areas and their minimum and maximum height limits

3.3 Neighbourhood Precinct Areas

3.3.1 North Residential Precinct

This area consists of the Shawmarr/Indian Heights and Credit Grove neighbourhoods. These stable residential areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

- a. The predominant characteristics of these areas will be preserved including: low rise building heights; the combination of small building masses on small lots; the well-landscaped streetscapes; and the regular street grid;
- b. New development is encouraged to reflect 1 to 2 storey building heights and should not exceed 3 storeys;
- c. Properties fronting onto Lakeshore Road West will complement the adjacent Mainstreet Precinct by continuing the 4 storey height limit; and

- d. Existing industrial uses along the railway tracks are encouraged to improve their transition to the adjacent residential areas. The maximum permitted height for industrial buildings (e.g. warehousing, self storage, etc.) will be the equivalent to a 2 storey residential building. New development will include appropriate buffers which ensure lighting, noise levels, loading and garbage areas do not negatively impact adjacent residential areas. Development will provide a streetscape that is compatible with adjacent neighbourhood residential uses.

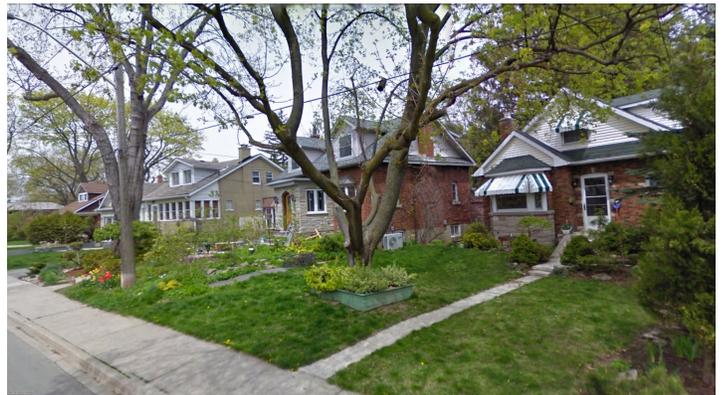


Figure C7 — Examples of dwellings within the Port Credit North Residential Neighbourhoods



Figure C8 — Examples of dwellings within the Port Credit North Neighbourhoods

3.3.2 South Residential Precinct

This area consists of the Cranberry Cove and Hiawatha neighbourhoods. These stable residential areas will be maintained while allowing for infill which is compatible with and enhances the character of the area.

- a. The predominant characteristics of these areas will be preserved including: the low rise building heights; combination of small building masses on small lots; physical and visual access to Lake Ontario from parks and the terminus of streets; the well landscaped streetscape and street grid pattern;
- c. New development will have a maximum height generally equivalent to 2 storeys; and
- d. Properties fronting onto Lakeshore Road West will complement the adjacent mainstreet precinct by continuing the 4 storey height limit.

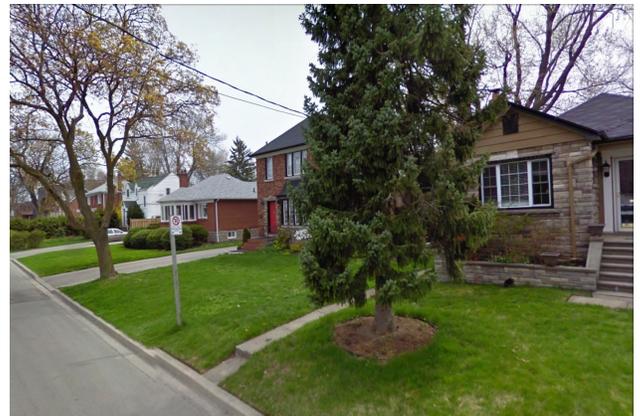


Figure C9 — Examples of dwellings within the Port Credit South Residential Neighbourhoods



Figure C10 — Examples of dwellings within the Port Credit South Neighbourhoods



3.3.3 Neighbourhood Mainstreet Precinct

The lands fronting onto Lakeshore Road East and West outside of the Port Credit Community Node are known as the Neighbourhood Mainstreet Precincts. These lands will be developed as a mixed use area.

- a. Heights along the Lakeshore Road Corridor will be a minimum of 2 storeys and a maximum of 4 storeys;
- b. Along Lakeshore Road East and West, mixed-use developments with street-related retail commercial uses and a rhythm of closely spaced storefronts lining the street will be required to foster an active pedestrian realm and to minimize the apparent width of Lakeshore Road East and West;
- c. Developments along Lakeshore Road East and West will be close to the street and have a setback of 0.6 m to 3.0 m. The appropriate setback will be determined through a detailed analysis of the public realm and streetscape requirements. Additional setbacks may be required to ensure an appropriate pedestrian realm;
- d. Main entrances of buildings will be located along the Lakeshore Road East and West frontage;
- e. Parking will be provided on street or in small, distributed parking lots or at the rear of the building through laneways. It is critical that parking facilities be located and designed to be compatible with the mainstreet character of the area by including measures such as landscape space, planters or other elements which activate the street wall and enhance public amenity;
- f. Parking, loading and service areas will be located behind the buildings fronting Lakeshore Road East and West and will be screened from view from residential properties to the north and south;
- g. New access points along Lakeshore Road East and West will be strongly discouraged. Where alternatives are not feasible, access will be consolidated between properties and preferably located where a traffic signal opportunity is feasible;
- h. Where development is proposed adjacent to residential neighbourhoods, attention should be given to ensuring appropriate transition;
- i. The assembly of adjacent low density residential land to enlarge properties fronting the Mainstreet Precinct is discouraged. However, should assembly occur, the primary purpose of these lands should be for buffering the adjacent residential uses and for amenity space and/or parking if required through the development;

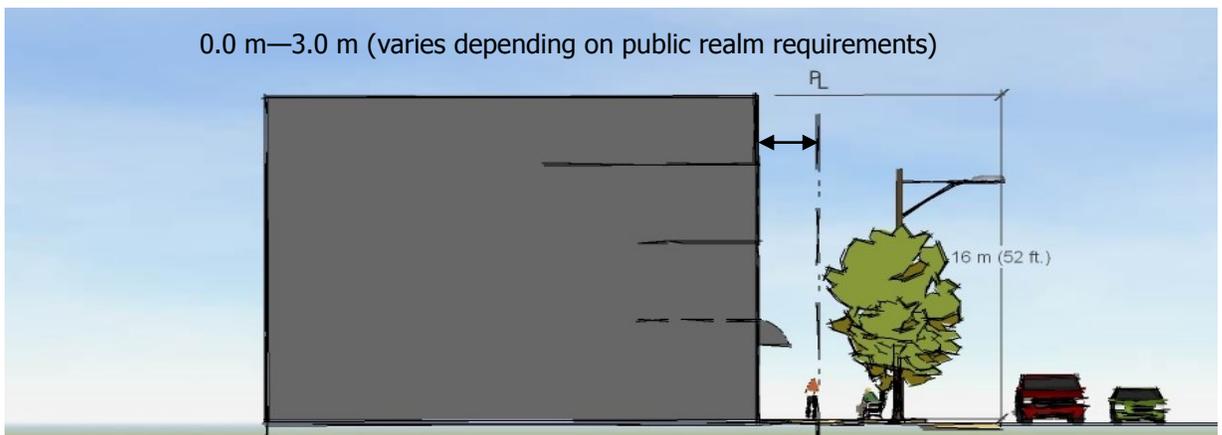


Figure C11 — Illustration of the setback on Lakeshore Road West and East

- j. Developments should maintain existing view corridors to the lake, such as along Oakwood Avenue South, and create new view corridors, such as, along the north/south public roads and
- k. Future development will recognize the character of adjacent land uses.



Figure C12 — Example of potential building heights on Lakeshore Road West and East

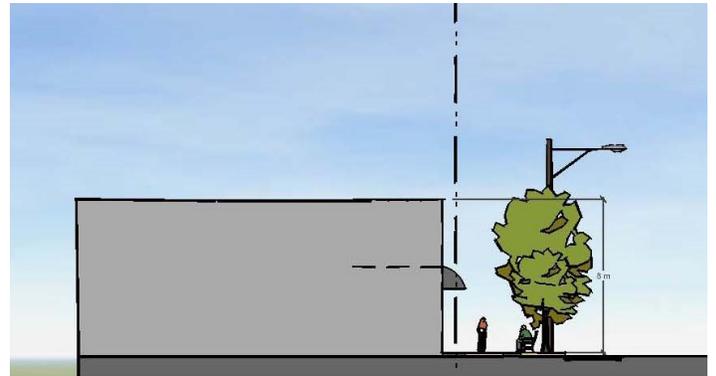


Figure C13 — Minimum building heights on Lakeshore Road West and East

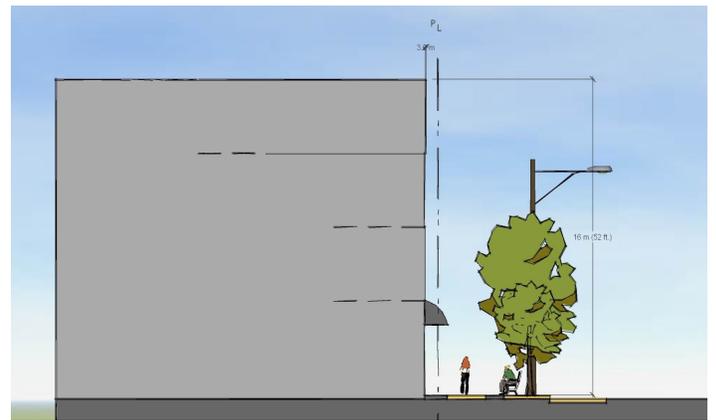


Figure C14 — Maximum building heights on Lakeshore Road West and East

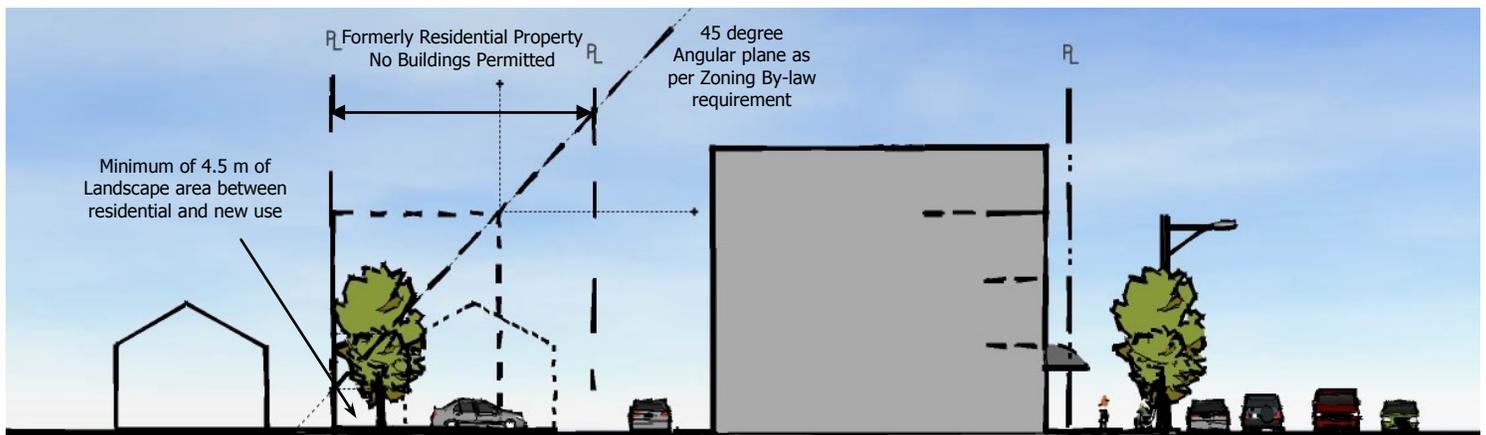


Figure C15 — Illustration of how the assembly of residential lands will be treated

3.3.4 Historic Village of Port Credit Precinct

This Precinct (also called the Historic Village of Port Credit) is a mixture of housing, retail commercial and community buildings of many types, representing different eras. While some of the housing stock is relatively new, the neighbourhood contains pockets of housing that dates back to the nineteenth century, creating a pleasing sense of "time depth". The Old Port Credit Village Heritage Conservation District Plan applies to lands within this precinct.

- a. Any additions, alterations, adaptive re-use or redevelopment will address how the development:
- Displays massing and scale sympathetic to surroundings;
 - Preserves the historic housing stock;
 - Supports the existing historic character;
 - Maintains the existing street grid pattern and building setbacks;
 - Maintains and enhances significant groupings of trees and mature vegetation.



Figure C16 — Example of structures in the Historic Village of Port Credit



Figure C17 — Example of structures in the Historic Village of Port Credit



Figure C18 — Example of structures in the Historic Village of Port Credit

3.3.5 Vacant Former Refinery Precinct

This precinct has redevelopment potential, however, further study is required to determine the appropriate development. This precinct should ultimately be developed in a manner which is compatible with the surrounding lands and which does not detract from the planned function of the Community Node.

- a. Building heights will provide appropriate transition to adjacent South Residential and Old Port Credit Heritage Conservation District Precincts;
- b. Public roads will be provided in any future development that separates any new development from the Lake Ontario Waterfront Open Space and the continuous waterfront trail to maximize public access to and along the Lake Ontario Waterfront; and
- c. A landscape buffer will be provided between the precinct and the adjacent residential neighbourhood to the west.
- d. The Vacant Former Refinery, along with associated lands located in the Neighbourhood Mainstreet Precinct are subject to further study to determine appropriate heights, built form and land uses.



Figure C19 — Imperial Oil Lands, waterfront path



Figure C20 — Imperial Oil Lands, waterfront path, old pier



Figure C21 — Imperial Oil Lands, View Looking north from the public pathway

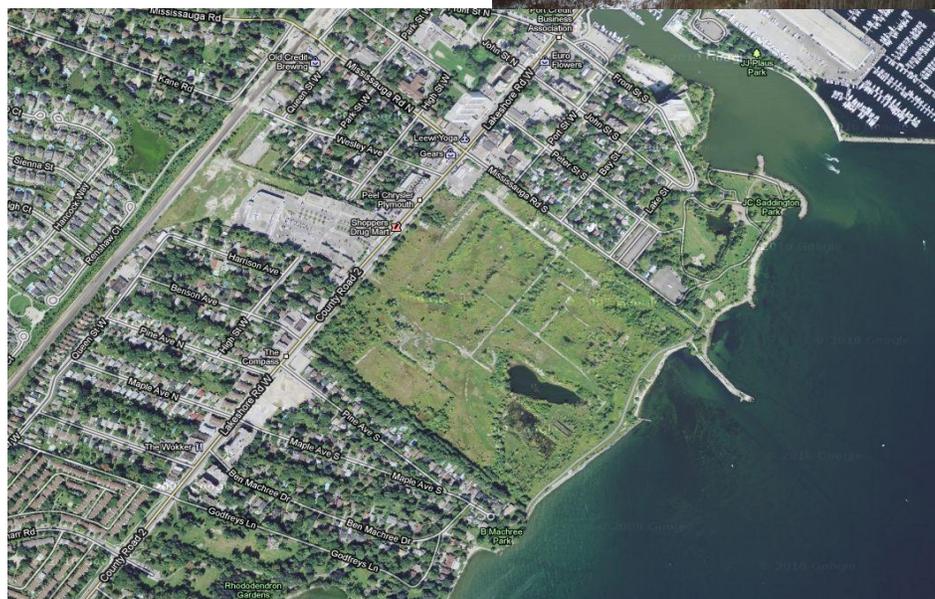


Figure C22 — Imperial Oil Lands

3.4 Views

Views to Lake Ontario are vital and should be enhanced where possible.

New development should ensure views to Lake Ontario within the Port Credit Neighbourhoods are maintained.



Figure C42 — Hiawatha Parkway and Onaway Road, view to Hiawatha Park

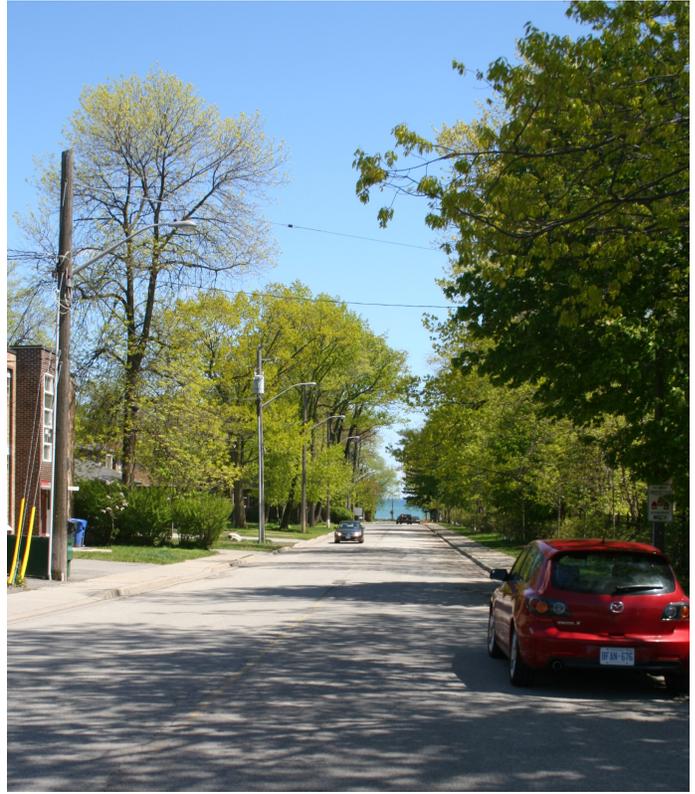


Figure C43 — Elmwood Avenue South

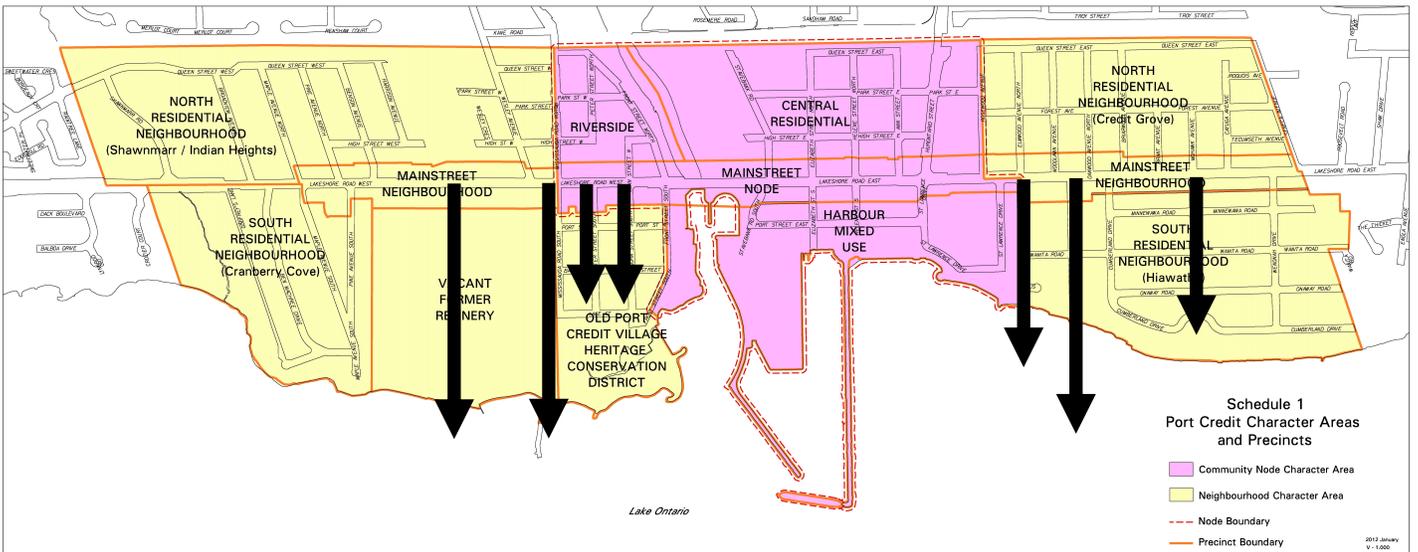


Figure C44 — Views to Lake Ontario

3.5 Cultural Heritage Resources

There are a number of heritage resources in the Port Credit Neighbourhoods. Designated properties are to be retained. The retention and enhancement of heritage listed properties is strongly encouraged.

New development proposed in proximity to these buildings should be developed in a manner that is sensitive and complementary to the character of the structure.

New developments should provide the appropriate transition to properties listed on the heritage register and in the Historic Village of Port Credit Precinct.



Figure C45 — 44 Wesley Avenue



Figure C46 — 34 Peter Street



Figure C47 — 5 Oakwood Avenue North



Figure C48 — 305 Lakeshore Road West Designated



Figure C49 — 11 Oakwood Avenue North

3.6 Parking, Servicing and Loading

The design of parking, servicing and loading areas is a key component in the development of sites. These areas serve a functional need, but should be designed in a manner that screens less desirable aspects and provides high quality treatment of exposed areas while addressing safe and efficient movement of pedestrians, cyclists and vehicles.

Parking should be located underground, internal to the building or to the rear of the buildings.

Above grade parking structures should be designed in such a manner that vehicles are not visible to the public and have appropriate directional signage to the structure .

Service, loading and garbage storage areas should be integrated into the building or located at the rear of the building and screened from the public realm and adjacent residential uses.



Figure C50 — Briarwood Avenue north of Lakeshore Road East

3.7 Access Points

The consolidation and relocation of access points along Lakeshore Road East and West will be encouraged to promote pedestrian safety and a cycle friendly environment to provide the opportunity for a continuous streetscape.

Mutual access between abutting properties will be essential in achieving a pedestrian oriented environment. Existing access will be relocated whenever possible to side streets and the rear of the property. The addition of access points will not be permitted.



Figure C51 — Pedestrian Realm with no access points



Figure C52 — Existing Access Points

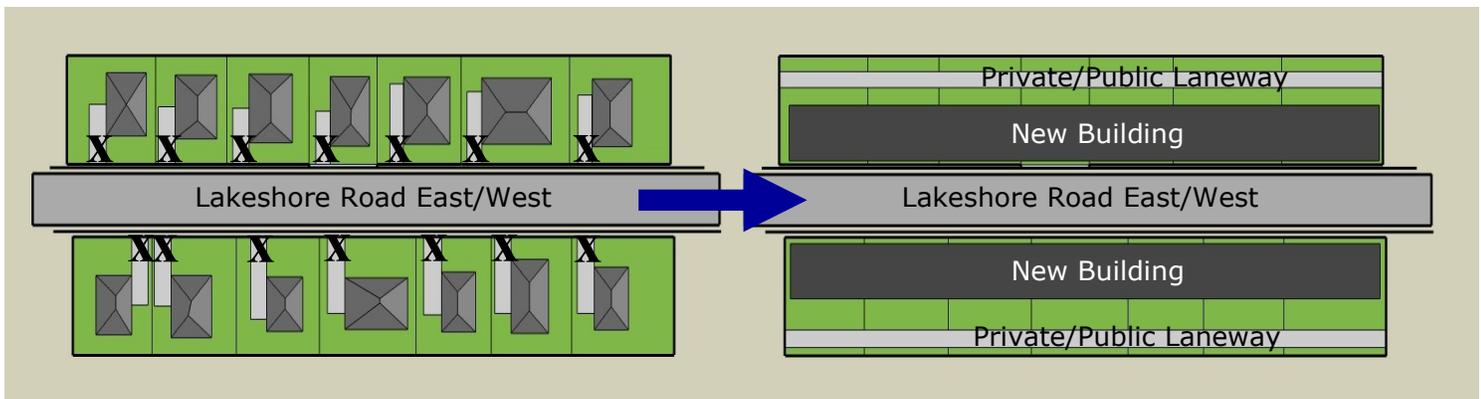


Figure C53 — Conceptual consolidation of access points along Lakeshore Road West and East

3.8 Pedestrian Realm/Streetscape

The Neighbourhood Precincts have an established streetscape particularly in the residential areas. These consist of mature tree lined streets and some sidewalks on the street edge. A significant number of the residential streets, however, do not have sidewalks. This will be maintained.

In the Neighbourhood Mainstreet Precinct, a Master Streetscape Plan will be required to determine the appropriate streetscape and to ensure any planned streetscape work takes into consideration established and planned utilities. Coordinated street furniture, including post and ring bike parking, should be established through this process.

In the interim, applicants for development applications along mainstreets will be required to submit a streetscape plan which clearly demonstrates how they can achieve the optimal boulevard width of 5.6 m and how the proposed development will fit in with the current streetscape pattern.



Figure C54 — Residential street in Port Credit



Figure C55 — Neighbourhood Mainstreet Precinct



Figure C56 — Existing pedestrian realm in the Neighbourhood Mainstreet

3.9 Microclimate

Shadow and wind comfort studies will be required as per the City of Mississauga Design Guidelines and Reference Notes.

3.9.1 Shadow Impact

Shadow studies will be requested in support of development applications to demonstrate that the height and/or location of a proposed building will not generate excessive shadows over adjacent lands.

Shadow studies will be required for buildings greater than 10.7 m in height which may cause new shadow impacts on adjacent residential properties, public parkland, open space and the public realm. Particular attention will be focused on the mainstreet corridors (Lakeshore Road East or West and Hurontario Street) where a pedestrian environment is strongly encouraged and fostered.



Figure C57 — Shadow impact on the Neighbourhood Mainstreet Precinct

3.9.2 Wind Comfort

Wind studies may be requested for developments over 3 stories or 16 m in height to ensure appropriate comfort and safety levels are maintained in the pedestrian realm, streetscapes, public spaces, private public amenity areas and areas immediately adjacent to and surrounding the proposed development. The study may be required on development applications higher than 3 storeys.

Evaluation of the existing wind conditions in the immediate and surrounding area, prior to the proposed development will be required along with a comparison of the wind conditions .

The criteria to be used for the analysis should be signed and sealed by a certified engineer. The *Terms of Reference for Pedestrian Wind Comfort and Safety Studied* should be used.



Figure C58 — Wind impact on the pedestrian environment

3.10 Materiality

As in the Community Node, new buildings should reference their surroundings through their architectural language and high quality materials. In addition, there may be other City initiatives and directions (e.g. Relevant Urban Design Guidelines, Port Credit Village Heritage Conservation District Plan, among some) which need to be consulted.

High quality building materials will be required in all new developments in Port Credit.

The predominant exterior building material is a reddish tone

brick and stone. New buildings are required to maintain this theme along the neighbourhood mainstreet and in the residential neighbourhoods.

For the mainstreet, vision glass should be used for all store fronts. Spandrel glass will not be permitted along Lakeshore Road East and West.

Concrete block is not permitted to be exposed. Painted concrete block is not permitted.



Figure C60 — Example of brick and stone material



Figure C61 — Example of brick and stone material



Figure C62 — Example of brick and stone material



Figure C63 — Example of brick and stone material / Example of roof feature



Figure C64 — Example of brick and stone material / Example of Canopy



**Environmental
Sustainability**

Environmental Sustainability

New developments must be sustainable in all aspects, taking into account social and economic impact, based on whole life costs and benefits. Advances in construction technology combined with a growing body of architectural knowledge mean that sustainable practices should be easily achievable.

On July 7, 2010, City Council adopted the Green Development Strategy that focuses on achieving sustainability and environmental responsibility in new development in Mississauga. The City strongly encourages applicants to incorporate green sustainable elements into proposed buildings, site works, construction methods and long-term maintenance programs. Further, the City also encourages that applicants pursue LEED-NC credits required to achieve Silver certification.

For More information, Visit Canada Green Building Council for the LEED – NC program, CVC/TRCA website for the Low Impact Development Stormwater Management Planning and Design Guide, City of Mississauga for the Green Development Strategy and Stage One Development Standards and Mississauga Water Quality Control Strategy.



Figure D1 — Example of vertical parking grate screened by landscape



Figure D2 — Example of enhanced dry grass swale



Figure D3 — Typical rain water barrel



Figure D4 — Extensive Green Roof, Mountain Coop, Toronto

Appendix A - Photo Credits

Page	Description	Credit
Cover	Small Upper: 10 Restaurant, Port Credit	Mississauga Image Library
	Small Lower: Detached dwellings in Port Credit	Mississauga Image Library
	Snug Harbour	Sharon Mittmann, City of Mississauga
Page 2	In order from top to bottom:	
	Port Credit Lighthouse	John Carvalho
	Ten Restaurant, Port Credit	Mississauga Image Library
	Second Cup, Port Credit	Sharon Mittmann, City of Mississauga
	Lakeshore Road West	Sharon Mittmann, City of Mississauga
	1 Port Street	Sharon Mittmann, City of Mississauga
	Clark Hall, Port Credit	Sharon Mittmann, City of Mississauga
	St. Lawrence Development	Sharon Mittmann, City of Mississauga
	Port Credit local Road	Sharon Mittmann, City of Mississauga
	Port Street East	Sharon Mittmann, City of Mississauga
Page 3	In order from top to bottom	
	Live Work Units on Lakeshore Road West	Mississauga Image Library
	Port Street	Sharon Mittmann, City of Mississauga
	Mainstreet Development on Lakeshore Rd. W.	Sharon Mittmann, City of Mississauga
	Central Residential Area	Sharon Mittmann, City of Mississauga
	High Street East	Sharon Mittmann, City of Mississauga
	Central Residential Area	Sharon Mittmann, City of Mississauga
	St. Lawrence Park	Sharon Mittmann, City of Mississauga
	1 Port Street	Sharon Mittmann, City of Mississauga
Lakeshore Road East	Sharon Mittmann, City of Mississauga	
Page 4	From top left	
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	Port Credit Go Sign	Sharon Mittmann, City of Mississauga
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Page 7	Central Residential Precinct Image off of Port Street	Sharon Mittmann, City of Mississauga
	Central Residential Precinct Image Heritage Building	Sharon Mittmann, City of Mississauga
	Riverside Precinct Image	Sharon Mittmann, City of Mississauga
	Central Residential Precinct Image	Mississauga Image Library
	Main Street Corridor Precinct (Commercial Area) Image	Sharon Mittmann, City of Mississauga
Page 8	Harbour Mixed Use Precinct Image	Sharon Mittmann, City of Mississauga
	Existing building heights in Port Credit	Geomatics, City of Mississauga
Page 9	Riverside Precinct	Sharon Mittmann, City of Mississauga
	Main Street Corridor Precinct (Commercial Area)	Sharon Mittmann, City of Mississauga
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Page 11	Differences in Building Heights. Buildings with the same number of floors are approximately 13% taller now.	Sharon Mittmann, City of Mississauga

	Building Transition from the Railway Tracks to Lakeshore Road East and West. Buildings should also transition from Lakeshore Road East and West to Lake Ontario with the exception of key landmark locations (e.g. Intersection of Hurontario Street and Lakeshore Road East)	Sharon Mittmann, City of Mississauga
Page 12	Mainstreet Precinct, Mississauga Road and Lakeshore Road West. New construction.	Sharon Mittmann, City of Mississauga
Page 13	Building heights in the Central Residential Precinct	Sharon Mittmann, City of Mississauga
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	Market Square Development, Lakeshore Road East	Mississauga Image Library
	Port Credit Node waterfront view	David Broderick, City of Mississauga
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	Image of the Riverside Precinct	Dan Magee, City of Mississauga
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	Image of the Central Residential Precinct	Dan Magee, City of Mississauga
	Image of the Central Residential Precinct	Dan Magee, City of Mississauga
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	Building separation distances on High Street East	Sharon Mittmann, City of Mississauga

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	Residential setbacks should be well landscaped. Setbacks should be a minimum of 7.0 m	Sharon Mittmann, City of Mississauga
	Sidewalk curb to face of the building for new developments will be 5.6 m	Sharon Mittmann, City of Mississauga
	Residential setbacks should be well landscaped. Setbacks should be a minimum of 7.0 m	Sharon Mittmann, City of Mississauga
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	90% of the building frontage is 0.6 m to 3.0 m	Sharon Mittmann, City of Mississauga
	Illustration of a minimum of 90% of the building shall front onto Lakeshore Road East and West and be within 0.6 m to 3.0 m	Sharon Mittmann, City of Mississauga
Page 31	70% of the frontages along Port Street East shall be 0.6 m to 3.0 m if no residential uses are proposed.	Google Maps
	70% of the building frontage should be 4.5 m to 7.5 m if residential units are proposed.	Google Maps
	Illustration of a minimum of 70% of the building shall front onto Port Street East and be within 0.6 m to 3.0 m	Sharon Mittmann, City of Mississauga
Page 32	30% landscape area for a small lot on a 40 m x 45 m lot size produces the minimum landscape buffer requirement of 4.5 m on the rear and side lot line. It does not maintain the required front and exterior side yard setback from the front property line.	Sharon Mittmann, City of Mississauga
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Page 41	Balconies should be designed so that they are part of the building rather than added onto the building as an afterthought. Glass balconies should have 50% tinting to ensure items on the balcony are not fully exposed.	http://www.beltcolombia.info/images/Vert_balconies4.jpg http://farm1.static.flickr.com/235/451194631_238d4ae797_o.jpg
	Concrete Block	Google Images
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	Stucco (synthetic or natural)	Google Images
	EIFS Panels should not be used for more than 20% of the building material and should not be placed within the first 4 storey's	Google Images
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	St Lawrence Park lookout	City of Mississauga Image Library
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